



Gravelly Beach



Exeter



Blackwall



Lanena

DRAFT Exeter and District Structure Plan 2023

**Document Control:**

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# 1 Introduction

The Draft Exeter and District Structure Plan 2023 builds on the foundations of the current Exeter Structure Plan. The current Exeter Structure Plan was adopted in 2014 and provided a 20 year vision for the area.

The Structure Plan has been reviewed and updated to understand what has happened in the eight years since the original Structure Plan was released and what the future needs of the community might be. The focus of the Draft Structure Plan is on land use and development and seeks to address future needs for growth, particularly residential growth. Other community priorities will be managed through separate processes.

## *Planning response*

Throughout the draft Structure Plan there are information boxes that list the 'Planning response' to the information presented. This has informed the Structure Plan and the requirements for use and development in section 6.

The recommendations represent strategic intent and may be subject to future funding commitments and approval of other authorities.

## 1.1 What is a Structure Plan?

A Structure Plan is a document that guides the future development of a town or suburb. It lays the foundation for regulatory zoning and the planning process including where future growth is planned to occur.

Land use planning in Tasmania sits within the Resource Management and Planning System as shown in Figure 1. Local land use planning is regulated through the Tasmanian Planning Scheme, with the Local Provisions Schedule reflecting the unique qualities for the municipality. The regulatory land use planning matters are usually incorporated into the Local Provisions Schedule through zones, overlays or specific area plans.

While there are no legislative requirements for the content or preparation of Structure Plans, amendments to the West Tamar Local Provisions Schedule will likely be required in order to effectively implement the outcomes promoted by the Structure Plan.

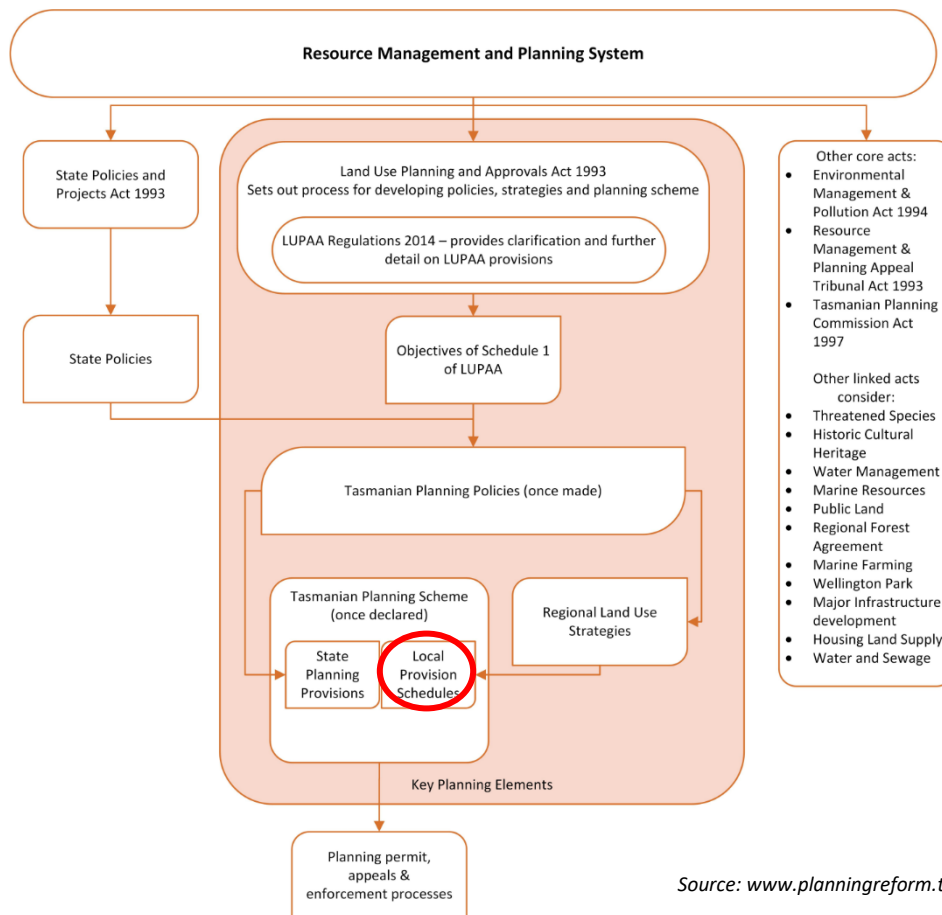
The Structure Plan has been prepared to provide the local planning policy which would support an amendment. It has been prepared to align with:

- The objectives of the Resource Management and Planning System; and
- The 'LPS criteria' included in section 34 of the *Land Use Planning and Approvals Act 1993*.

### The objectives of the resource management and planning system of Tasmania are –

- (a) to promote the sustainable development of natural and physical resources and the maintenance of ecological processes and genetic diversity; and
- (b) to provide for the fair, orderly and sustainable use and development of air, land and water; and
- (c) to encourage public involvement in resource management and planning; and
- (d) to facilitate economic development in accordance with the objectives set out in paragraphs (a), (b) and (c); and
- (e) to promote the sharing of responsibility for resource management and planning between the different spheres of Government, the community and industry in the State.

*Source: Schedule 1, Land Use Planning and Approvals Act 1993*



Source: [www.planningreform.tas.gov.au](http://www.planningreform.tas.gov.au)

Figure 1: Tasmanian Planning System at a Glance

### 1.3 Review of the 2014 Structure Plan

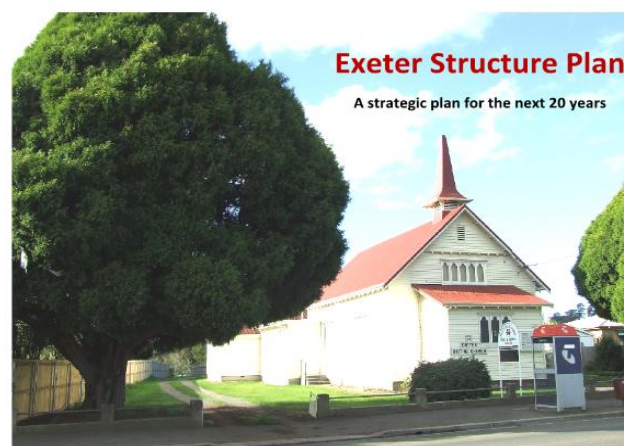
The review was informed by initial consultation with the community to understand:

- What do you like most about Exeter?
- What improvements do you think could be made in Exeter?
- What does the Exeter Community need?

Over 30 submissions were received which informed the development of the Draft Exeter and District Structure Plan 2023.

A summary of the feedback received and a response is included in Appendix 2.

Land use planning matters will be addressed in the draft Structure Plan where planning intervention is required. Other matters will be considered and addressed as detailed in Appendix 2.



2014 Exeter Structure Plan

### 1.3.1 What we like most about Exeter

Initial consultation provided feedback about what participants liked most about Exeter with a sample of responses provided below.

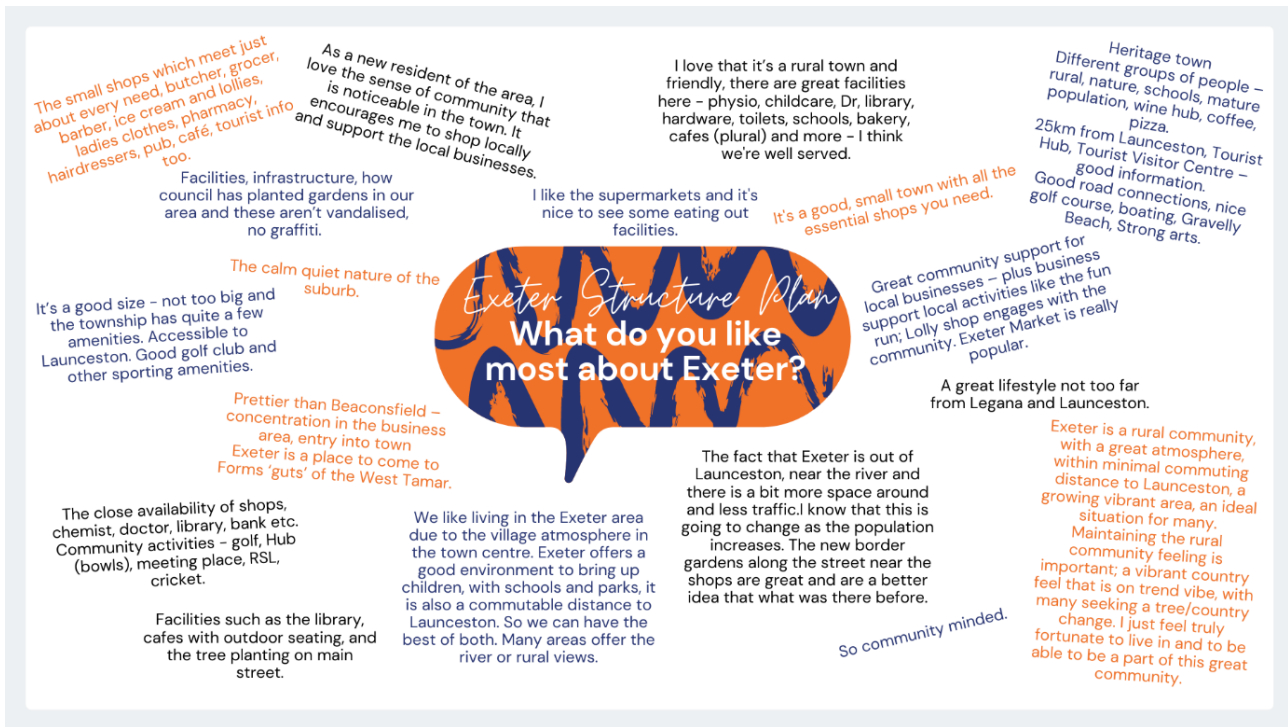


Figure 2: Summary of community consultation about what people like most about Exeter

A workshop was held with around 25 Exeter High School students on Friday 26 March 2021. Students were asked to consider what they liked most about Exeter and what would they like to change or improve. Figure 3 summarises what they said.

**Students were also asked to report their top ideas for improvements to Exeter:**

- Parking
- Walks, pathways and crossings
- Dog park, recreation, walking trail etc behind Primary School
- Solar panels
- Roundabout at end of Glen Ard Mohr Road
- Road from Frankford Road to Gravelly Beach
- Entertainment
- Trail behind community hub
- Redo park and goat track
- Youth centre
- Pump track park behind Community Hub
- Crossing near School farm.



Figure 3: Exeter High School Workshop summary - what they liked most about Exeter and what would they like to change or improve

The consultation process also identified a need to extend the Structure Plan area to include Gravelly Beach, Blackwall and Lanena as the broader Exeter community. People in the district identify with Exeter and have a strong connection with and reliance on Exeter.

### 1.3.2 Opportunity sites

The 2014 Structure Plan identified six opportunity sites as shown in Figure 4 below.



Figure 4: 2014 Structure Plan opportunity sites (Green – privately owned and Orange – Council owned)

Table 1 reviews these opportunity sites, what has been implemented since 2014 and considers if any interventions are required to be included in the draft Structure Plan.

Table 1: Review of the 2014 Structure Plan opportunities

The 2014 Opportunities	2022 Review
<p><b>Murray Street Community Precinct</b></p> <p>The Exeter Community Hub was completed and opened in 2019 and is the first stage of the Community Precinct. Longer term considerations include the potential relocation of Tresca services, the library and additional services such as medical facilities and community activities.</p> <p>The more recent establishment of a fenced off leash dog park, a bandicoot garden and maintenance of the goat track is providing the foundation for an important community precinct into the future.</p>	<p>Include the site in the Community Purpose Precinct in the new Structure Plan.</p> <p>Continue long term planning and investment to enhance the services provided.</p> <p>Request an amendment to the West Tamar LPS to include the site in the Community Purpose Zone.</p>
<p><b>Biloo Street Public Park</b></p> <p>The Biloo Street Park was established in 2019. It provides public facilities and a meeting and rest stop in Exeter for residents and visitors.</p>	<p>Investigations in relation to traffic movements and a potential caravan waste dump point in the vicinity of the park and Council’s works depot are ongoing.</p> <p>There are no land use planning impediments under the current planning scheme to</p>

## The 2014 Opportunities

## 2022 Review

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### Corner Frankford Road and Main Road

This is a group of four privately owned sites located at the western entry to Exeter. The 2014 Structure Plan provided limited commentary in relation to the opportunity for the site.

A Planning Permit has since been issued for multiple dwellings on two of the properties.

implement the above options and no additional planning interventions are required.

The site is included in the Local Business Zone which provides a high degree of flexibility for future commercial uses that would be suitable for the site.

Future road upgrades or street beautification works should take into consideration the role of this intersection as the western gateway to Exeter.

No additional planning interventions are required at this time.

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### 122 Main Road (Exeter Hotel)

The 2014 Exeter Structure Plan considered the potential for Council to consider purchasing this site to facilitate:

- better land management in relation to weed infestation with the interface with Stony Brook;
- future options for a road link and associated land development; and
- Council ownership of the car parking area fronting Main Road.

At this stage Council have not been in the position to purchase the site, however options in relation to a future road link are included later in the Structure Plan.

The site is included partly in the General Residential Zone and partly in the Local Business Zone.

There is potential for residential subdivision of around 20 lots as broadhectare land supply discussed in section 4.

As noted in the 2014 Structure Plan, the site is also important for achieving a connected road network and providing connections from Wildmore Crescent to Main Road and Murray Street. These infrastructure requirements are considered further in section 5.

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### 106 Main Road

This is a single site located between the Exeter Bakery and the Exeter Uniting Church. Limited detail is provided in the 2014 Structure Plan in relation to the opportunity, however given its Main Road location, and prominent position there are a range of commercial opportunities that may be suitable for the site.

The site is included in the Local Business Zone which provides a high degree of flexibility for future commercial uses that would be suitable for the site.

No additional planning interventions are required.

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### 185 Gravelly Beach Road

This site was highlighted for potential low density residential development with the opportunity to establish a link road between Glen Ard Mohr Road and Gravelly Beach Road (or potentially Murray Street).

Residential growth areas and investigation areas are further examined in Section 4.

*Planning response*

- Expend the Structure Plan area to include Gravelly Beach, Blackwall and Lanena.
- Future development of Exeter and District should retain the village atmosphere of Exeter.
- Include the Community Hub and associated land in the Community Purposes Zone to allow for expansion of activities in the Community Precinct and request an amendment to the LPS.
- Consider road connection improvements to Wildmore Crescent and Murray Street.
- Investigate and identify areas for future residential growth, including consideration of the area between Exeter and Blackwall.
- Consider future business activity needs to cater for local business growth and expansion.



## 2 Structure Plan Area

The 2014 Exeter Structure Plan focused on the Exeter Township as shown in Figure 5, however the recommendations included in the plan extended beyond this boundary.

Consultation completed as part of the review highlighted the community connection and relationship between Exeter and nearby Gravelly Beach, Blackwall and Lanena which, while recognised as places in their own right, form part of the broader Exeter community.

The scope of the Structure Plan has been extended to Exeter and District as shown in Figure 6. The boundary generally aligns with the Australian Bureau of Statistics SA1 (suburb) boundaries for Exeter, Gravelly Beach, Blackwall and Lanena.

### 2014 Structure Plan study area

*'The Exeter Structure Plan area the General Residential zoned land at the Gravelly Beach Road junction, the Local Business zone in the town centre and the General Residential and Low Density Residential areas to the north of the town. Key outlying sites were also included; Exeter Golf Club and the Tresca community centre, the sewage ponds and the sawmills on Frankford Road. Similarly, any issues or opportunity sites outside the study area that arose during the structure planning process were included.'*



Figure 5 - Extract from the 2014 Exeter Structure Plan



Gravelly Beach local businesses



Figure 6 – Exeter and District Structure Plan Area



*Rose Bay Park, Gravelly Beach*



*Bandicoot garden, Exeter*

### 3 About Exeter and District

Exeter and District is made up of the towns of Exeter, Gravelly Beach, Blackwall and Lanena.

Exeter is centrally located in the West Tamar Municipality, 24km north of Launceston. The town functions as a service centre for the surrounding areas of Lanena, Blackwall, Gravelly Beach, Rosevears and Deviot, along with the rural localities to the north and west of Glengarry, Frankford and Bridgenorth.

There are still reminders of the district’s fruit growing past in the landscape, whilst now the home of varied industries including retail, landscaping, metal fabrication, agriculture and food, a Primary and High School, Community Hub and Council’s Waste Transfer Station and Works Depot.

Gravelly Beach, located on the banks of kanamaluka/Tamar River estuary, has stunning views over the Tamar Valley. The low density residential area and rural hinterland which has traditionally been a holiday village, is now a residential community featuring local businesses, and the popular Rose Bay Park. Continuing south, Blackwall and Lanena are also benefitted by beautiful views and a quiet riverside lifestyle.

#### 3.1 Our community

This section examines how the Exeter and surrounding area community has changed in the 15 years between 2006 and 2021. Understanding these changes provides useful context to planning for the future.

##### 3.1.1 Population and housing

At the 2021 census, the population in the Structure Plan area was 1733. In the 15 years from 2006 this represents an average annual growth rate (AAGR) of 1.09% - see Table 2. Over the same period, the AARG for the West Tamar municipality was 1.47%.

Between 2016 and 2021 the AAGR was 2.4%, or an increase in 189 people across the Structure Plan area. This was consistent with the AAGR for West Tamar.

This is a significant increase in population compared to previous years representing a 12% increase in population.

*Table 2: Population (place of usual residence) 2006 to 2021*

Location	Year				
	2006*	2011	2016	2021	
Exeter	338	389 (+51)	387 (-2)	446 (+59)	
Gravelly Beach	536	561 (+25)	567 (+6)	641 (+74)	
Blackwall		306	270 (-36)	303 (+33)	
Lanena	615	313 (+4)	320 (+7)	343 (+23)	
<b>TOTAL</b>	<b>1489</b>	<b>1569 (+80)</b>	<b>1544 (-25)</b>	<b>1733 (+189)</b>	

Source: Australian Bureau of Statistics, Census of Population and Housing, 2006, 2011, 2016 & 2021 (Usual residence data) included in the Basic/General Community Profiles

Notes:

\* In 2006, Blackwall was part of the Lanena SLA. Between 2006 and 2011 Census the Exeter SLA reduced from 3.9sq km to 3.2sq km and Gravelly Beach increased from 5.1sq km to 6sq km. The overall size of the combined structure plan area changed from 13.7 sq km to 13.8 sq km between 2006 and 2011.

The Census recorded an additional 137 occupied dwellings between 2006 and 2021 in the structure plan area – see Table 2. Almost 100 additional occupied dwellings were recorded in the 5 years between 2016 and 2021 with an AAGR of 3.3%. There is limited diversity in housing types with 99% of occupied dwellings in 2021 being separate houses. By comparison, separate houses represent 96% of occupied dwellings in the West Tamar municipality and 88% in Tasmania.

Council's building approvals data indicate only 51 new dwellings were approved in the same period – 31 of these were in Exeter which coincides with the approval of a 38 lot subdivision in 2017. This may indicate that some existing dwellings that were previously used for holiday homes or visitor accommodation are now being used for permanent accommodation.

Short stay accommodation data compiled by The Department of Justice<sup>1</sup> indicates approximately 20 properties in the Structure Plan Area are being used for short stay accommodation. 13 of these properties are also the primary residence of the provider (meaning a room or part of the residence is used for short stay accommodation) leaving only seven properties that are used only for visitor accommodation on a commercial basis. This data does not take into account a holiday home that is not commercially provided for visitor accommodation, for example, a family holiday house or shack.

The 2021 Census recorded 772 private dwellings in the Structure Plan Area with 77 unoccupied dwellings, or 9% vacancy rate. While this is higher than the number of short stay accommodation premises, there would be a number of people not home on census night, or, as above, some properties that are not permanently occupied. By comparison, just over 9% of dwellings in Australia were unoccupied on Census night<sup>2</sup>. This is not considered significant, particularly as coastal locations are popular for holiday homes and shacks.

The structure of dwellings has remained consistent with most dwellings being separate houses – see Table 3. Census data indicated there are very few multiple dwellings however it seems unlikely that the census recognises multiple dwellings that are detached units, and this is the most common form of multiple dwellings approved in the Structure Plan Area.

With 700 occupied dwellings recorded on the 2021 census night and a population of 1733 persons, the average household size in the Structure Plan Area was 2.4 persons. This has reduced from 2.6 persons per household in 2006. In Tasmania, the average household size in 2021 was 2.5 persons.

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<sup>1</sup> Based on Q1 2022 data provided by the Consumer, Building and Occupational Services unit of the Tasmanian Government Department of Justice. The *Short Stay Accommodation Act 2019* requires booking platform providers to collect and display certain information regarding short stay premises listed on their booking platforms.

<sup>2</sup> Australian Bureau of Statistics <https://www.abs.gov.au/statistics/people/housing/housing-census/latest-release>

Table 3: Dwelling structure of occupied private dwellings and total dwellings 2006 to 2021

Location	Dwelling structure of occupied dwellings*				Total dwellings***	
	Separate house	Multiple dwellings	Other	Total	Number	Percentage of unoccupied dwellings
<b>2006**</b>						
Exeter	140	0	4	144	..	..
Gravelly Beach	206	0	0	206	..	..
Blackwall	..	..	..	..	..	..
Lanena	206	7	0	213	..	..
<b>TOTAL</b>	<b>552</b>	<b>7</b>	<b>4</b>	<b>563</b>	<b>..</b>	<b>..</b>
<b>2011</b>						
Exeter	152	0	4	156	166	7%
Gravelly Beach	223	0	0	223	257	14%
Blackwall	107	0	0	107	121	12%
Lanena	114	0	3	117	135	13%
<b>TOTAL</b>	<b>596</b>	<b>0</b>	<b>7</b>	<b>603</b> (+40 or 7.1%)	<b>679</b>	<b>11%</b>
<b>2016</b>						
Exeter	152	0	4	158	171	8%
Gravelly Beach	209	3	0	212	245	13%
Blackwall	107	0	0	107	122	17%
Lanena	124	0	0	124	142	14%
<b>TOTAL</b>	<b>592</b>	<b>3</b>	<b>4</b>	<b>601</b> (-2 or -0.3%)	<b>688</b> (+9 or 1.3%)	<b>13%</b>
<b>2021</b>						
Exeter	185	5	0	188	208	10%
Gravelly Beach	245	3	0	248	273	9%
Blackwall	122	0	0	122	135	10%
Lanena	142	0	0	142	156	9%
<b>TOTAL</b>	<b>694</b>	<b>8</b>	<b>0</b>	<b>700</b> (+99 or 16.5%)	<b>772</b> (+84 or 12%)	<b>9%</b>

Source: Australian Bureau of Statistics, Census of Population and Housing, 2006, 2011, 2016 & 2021 (Dwelling Structure data) included in the Basic/ General Community Profiles (Table G36 of 2021, Table G39 of 2016, Table B36 of 2011 and Table B31 of 2006)

Notes:

Please note that there are small random adjustments made to all cell values to protect the confidentiality of data. These adjustments may cause the sum of rows or columns to differ by small amounts from table totals.

\* Multiple dwelling includes: Semi detached, row or terrace house, townhouse etc; flat or apartment

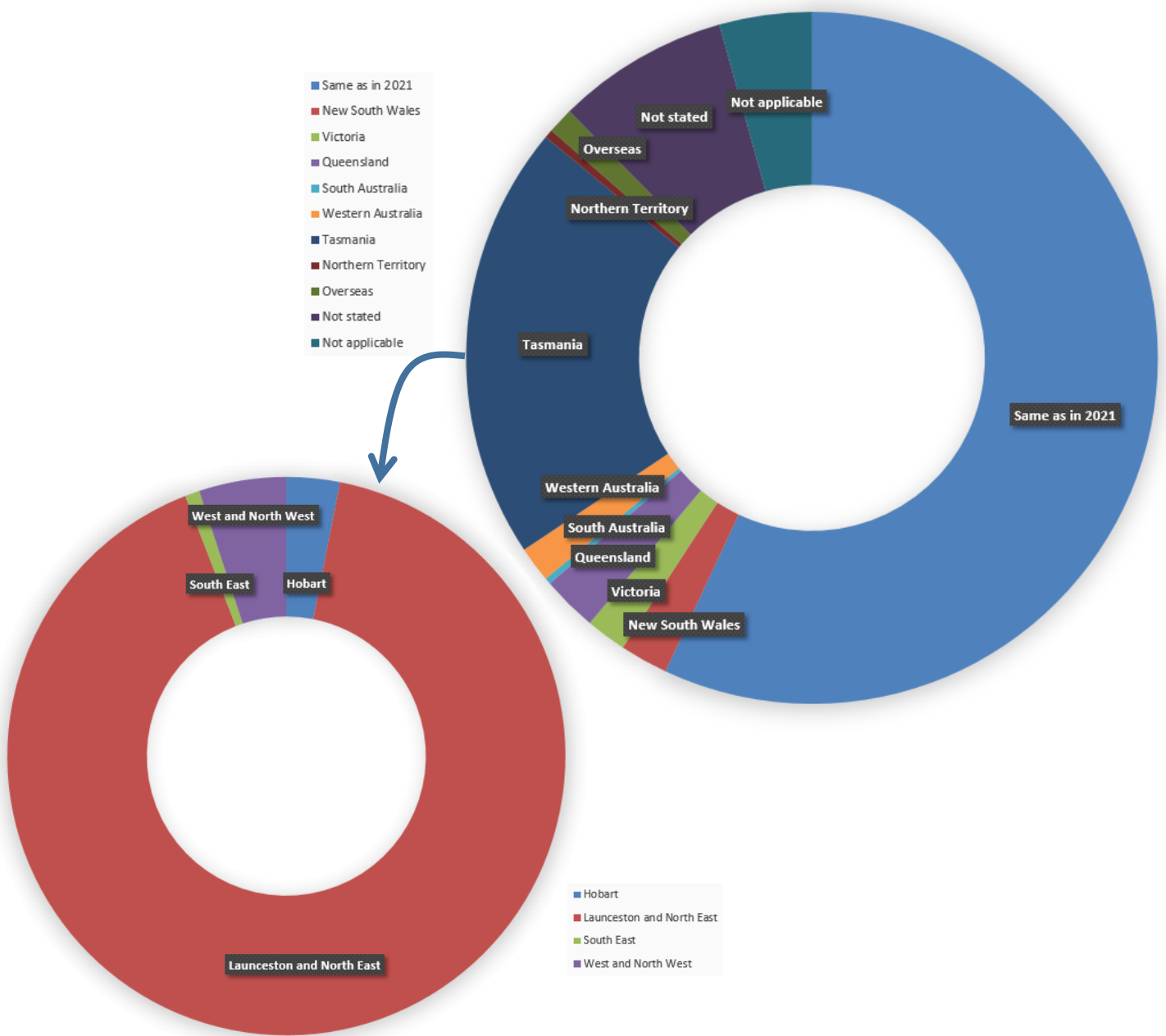
Other dwellings includes: Caravan, cabin, houseboat, improvised home, tent, sleepers out, house or flat attached to a shop or office, and not stated

\*\* In 2006, Blackwall was part of the Lanena SLA. Between 2006 and 2011 Census the Exeter SLA reduced from 3.9sq km to 3.2sq km and Gravelly Beach increased from 5.1sq km to 6sq km. The overall size of the combined structure plan area changed from 13.7 sq km to 13.8 sq km between 2006 and 2011.

\*\*\*Total dwellings was not recorded by the census in 2006.

57% of the residents in the Structure Plan area lived at the same address in 2021 as they did in 2016<sup>3</sup>. Most new residents moved from other places in Australia with only 1% from overseas. Of the 511 residents who lived somewhere else in Australia in 2016, 69% were living in Tasmania and 91% of those were in Northern Tasmania, with 55% from the West Tamar municipality – see Figure 7.

<sup>3</sup> Australian Bureau of Statistics 2021 Census of Population and Housing, Place of Usual Residence Five Years Ago

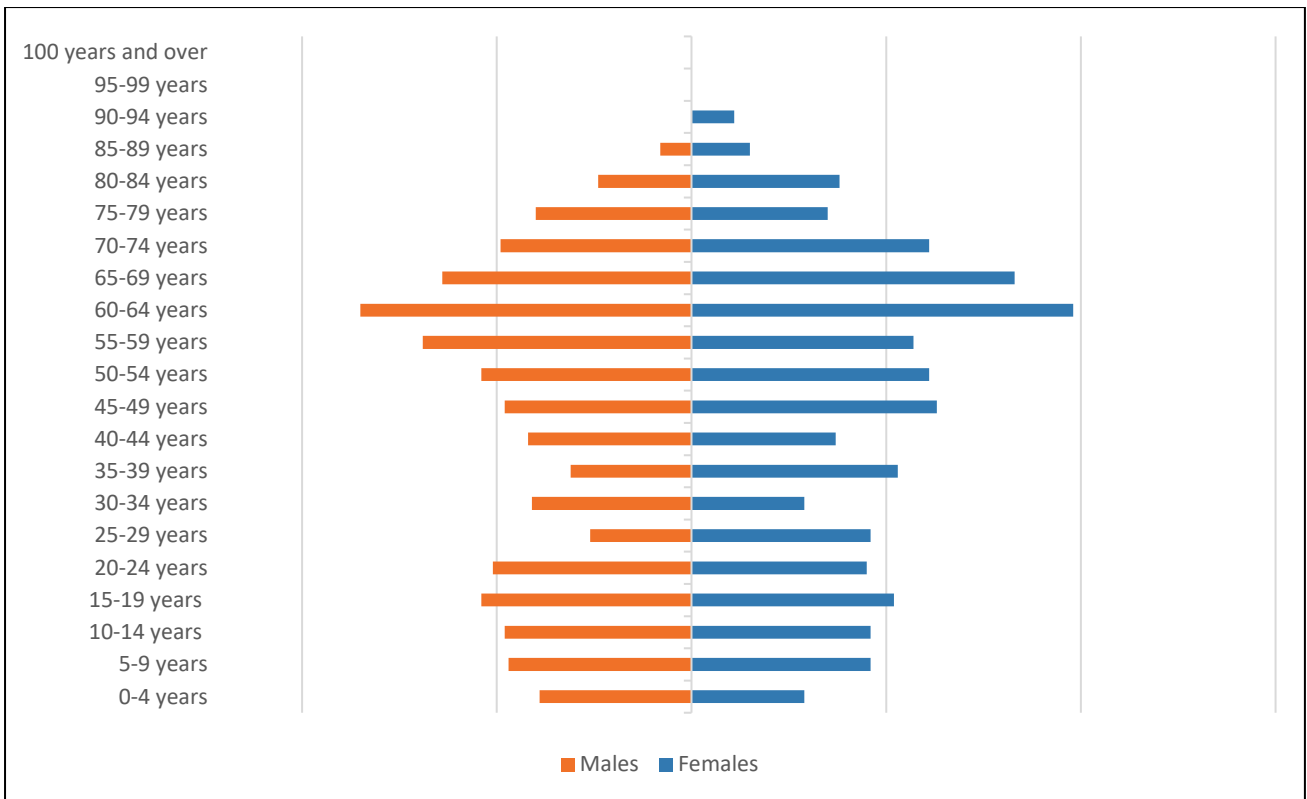


Source: Australian Bureau of Statistics 2021 Census of Population and Housing, Place of Usual Residence Five Years Ago

Figure 7 –Where residents of the Exeter Structure Plan Area at the time of the 2021 Census lived in 2016

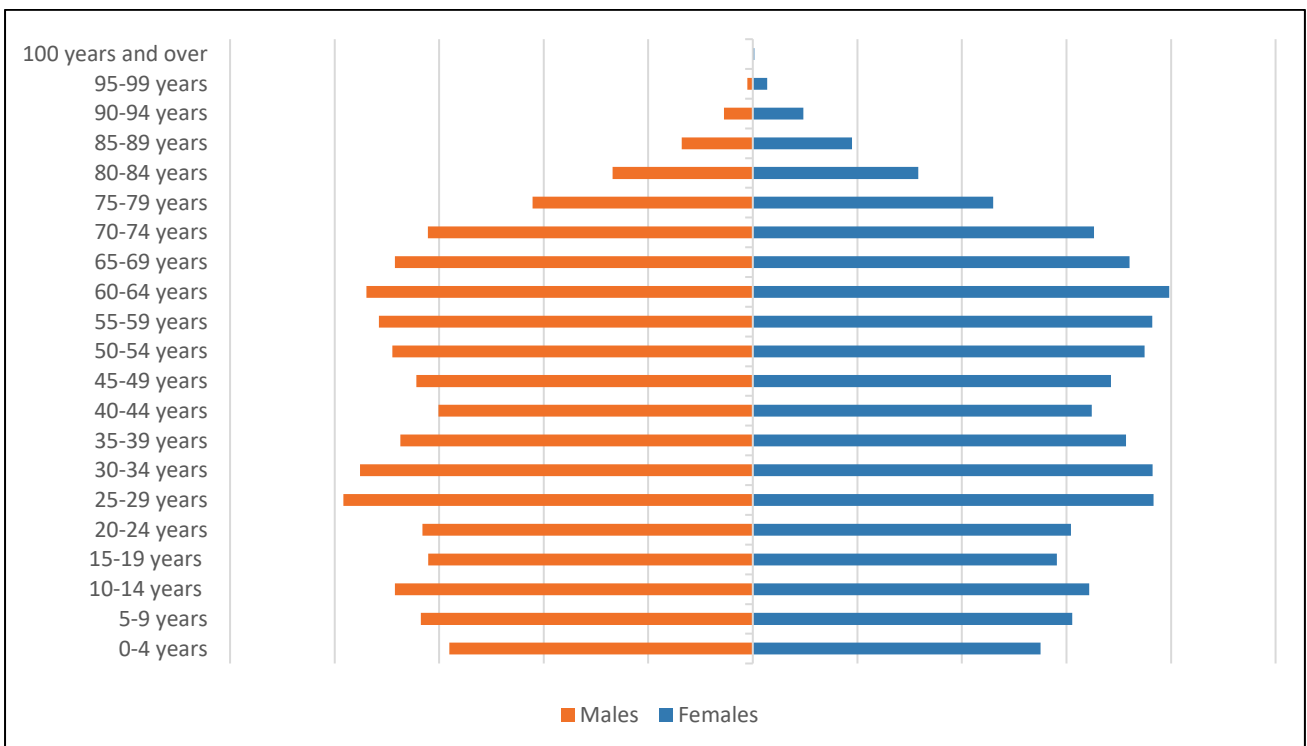
Almost 35% of the population in the Exeter Structure Plan Area were aged 60 years and over in 2021 as illustrated in Figure 8. By comparison, Tasmania has a more evenly distributed age profile with almost 28% of the population aged 60 years and over in 2021 as illustrated in Figure 9.

This aligns with the high proportion of lone person and couple households which made up 68% of occupied dwellings in 2021. The proportion of lone person households has been increasing since 2006 with households with three or more persons decreasing by the same proportion – see Table 4.



Source: Australian Bureau of Statistics 2021 Census of Population and Housing, Age by Sex, General Community Profile Table G04

Figure 8: 2021 Age Sex Pyramid – Exeter Structure Plan Area



Source: Australian Bureau of Statistics 2021 Census of Population and Housing, Age by Sex, General Community Profile Table G04

Figure 9: 2021 Age Sex Pyramid - Tasmania

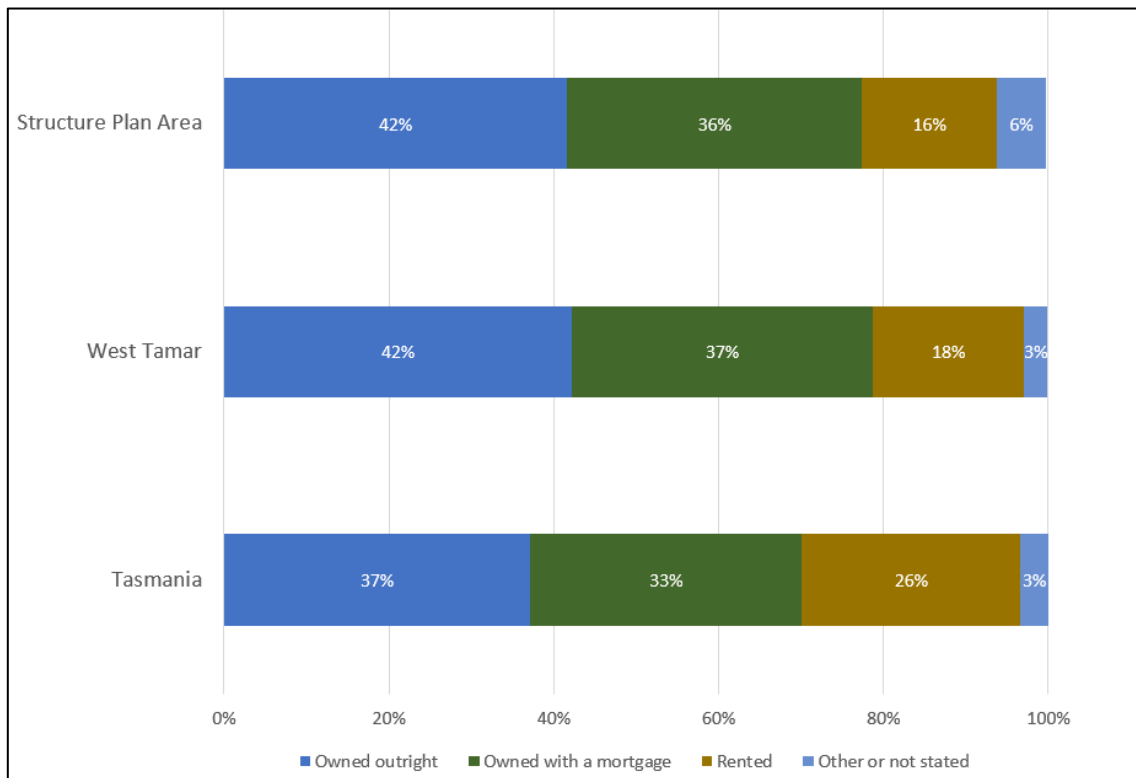
Table 4: Household structure (occupied dwellings) 2006-2021

Location	Lone person				2 persons				3 or more persons			
	2006	2011	2016	2021	2006	2011	2016	2021	2006	2011	2016	2021
Exeter	28%	33%	32%	37%	42%	34%	33%	35%	30%	33%	32%	32%
Gravelly Beach	25%	29%	31%	30%	36%	36%	37%	34%	36%	35%	34%	30%
Blackwall*	N/A	21%	32%	28%	N/A	40%	48%	43%	N/A	38%	24%	25%
Lanena	16%	19%	19%	18%	42%	56%	57%	53%	42%	25%	28%	29%
<b>Structure Plan area</b>	<b>22%</b>	<b>27%</b>	<b>29%</b>	<b>29%</b>	<b>40%</b>	<b>40%</b>	<b>42%</b>	<b>40%</b>	<b>37%</b>	<b>33%</b>	<b>31%</b>	<b>30%</b>
Tasmania	27%	27%	30%	29%	36%	35%	36%	36%	37%	34%	34%	35%
West Tamar	21%	23%	26%	26%	38%	39%	39%	39%	41%	39%	35%	35%

Source: Table B30 2006 Basic Community Profile; Table B30 2011 Basic Community Profile; Table G31 2016 General Community Profile; Table G35 2021 General Community Profile

\*Blackwall and Lanena were a single area in the 2006 Census

78% of dwellings in the Structure Plan Area are either owned outright or with a mortgage with 16% rentals. This is consistent with the West Tamar municipality however when compared to the whole of Tasmania there is a lower proportion of rental properties with 26% of Tasmania’s private dwellings being rented – see Figure 10. This indicates a relatively low availability of rental properties in the area.



Source: Australian Bureau of Statistics Census of Population and Housing - 2006 Basic Community Profile Table B32; 2011 Basic Community Profile Table B32; 2016 General Community Profile table G33; 2021 General Community Profile Table G37

Figure 10: Tenure of occupied private dwellings

Housing affordability is a measure of the percentage of household income used to secure housing. Generally if less than 30% of income is used on housing, this is considered affordable. For the 42% of households that own their property outright this is less of an issue as mortgage or rent payments are not required noting that other costs like rates and utilities are not accounted for.

As an indication of housing affordability, the percentage of the median household income used for mortgage or rent payments was calculated and depicted in Figure 11. It is acknowledged that half the community are paying more than this amount and there would be a reasonable percentage within the structure plan area where their housing would not be considered affordable. Based on the data, Exeter is the least affordable location. This may be indicative of the recent increase in land and construction costs as the majority of new dwellings have been built in Exeter. As outlined above, 57% of residents lived in the same place in 2021 that they did in 2016. If those dwellings were purchased prior to the housing price increases, then mortgages and rents would likely be lower than if purchased more recently. Nevertheless, it is important to continue to monitor housing affordability to ensure there is sufficient opportunity for affordable housing.

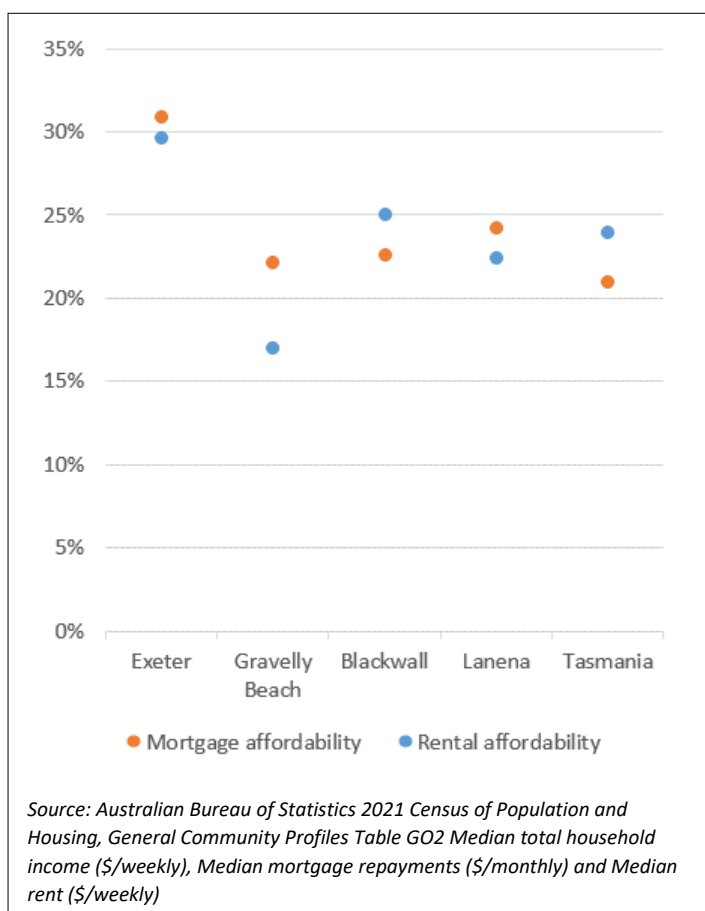


Figure 11: Housing Affordability indicator 2021  
 (% median mortgage repayments / rent to median household income)

Average sales prices for both non-vacant (with a dwelling or improvements) and vacant land have increased significantly across the four localities in the Structure Plan Area – See figure 12. The higher sales prices reduce the affordability for the area, however as a high proportion of residents are in older age cohorts, affordability may have less influence on people relocating in the area with established careers, downsizing or transitioning to retirement.

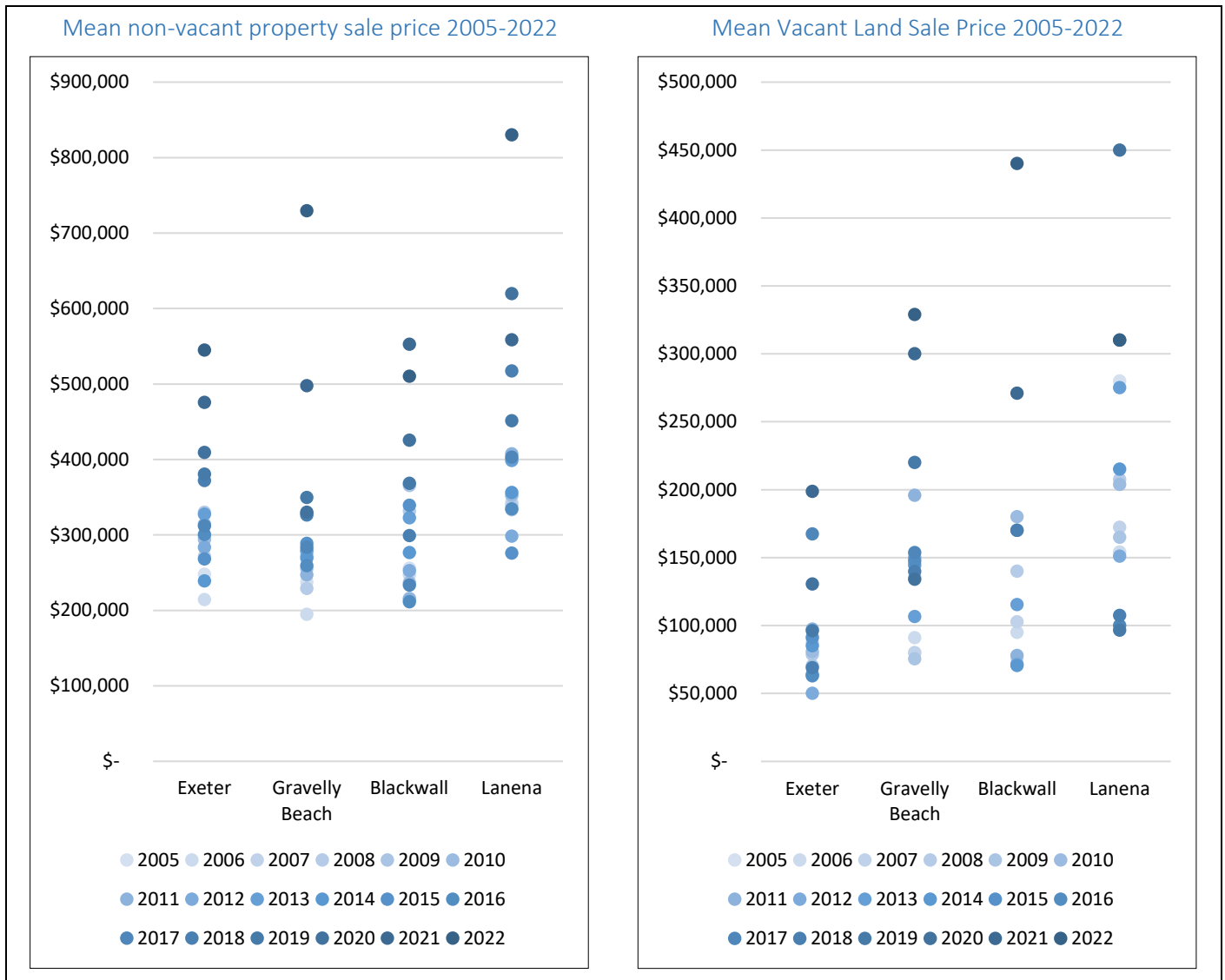


Figure 12: Mean property sale price 2005 to 2022

In summary, the Exeter and District community:

- Is growing faster than it has in the past;
- Is ageing and has a high proportion of single or couple households;
- Mostly lives in detached houses, however the number multiple dwellings has increased in recent years;
- Mostly own or are paying off their homes with lower numbers of rental properties; and
- House prices have increased significantly which may affect housing affordability.

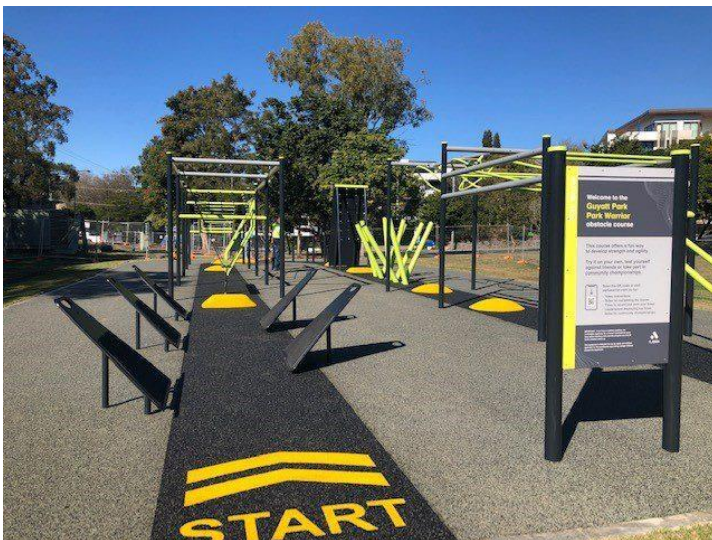
### 3.1.2 Community services and facilities

As Exeter is a regional service centre there are a large range of community services and business activities in the Structure Plan Area including those listed here.

The community has also highlighted opportunities for future public or private investment as:

- Parks and recreation that specifically caters for young people – for example a strength and agility obstacle course proposed by the West Tamar Youth Strategy 2023-2026 (example shown in Figure 13);
- Veterinary services;
- Additional food or retail offerings; and
- Expanded service offerings at the Community Hub.

While the Structure Plan cannot require investment, these services are promoted as priorities for future consideration.



source: [www.ospace.com.au](http://www.ospace.com.au)

Figure 13 - Strength and agility course installed in Brisbane

#### Some of the community services and business activities in the Structure Plan Area

##### Community

- Exeter Primary School (Kindergarten to year 6)
- Exeter High School (years 7 to 12)
- Childcare centre
- Council works depot and waste transfer station
- State Emergency Service (SES)
- Tamar Visitor Centre
- Exeter Police station
- Library
- Community Hub and RSL
- Tresca Community Centre
- Places of worship
- Off-leash dog area

##### Recreation

- Exeter Showgrounds (and AFL / cricket oval)
- Exeter Golf club
- Exeter Bowls club
- Squash courts
- Exeter Main Street park
- Coastal parks and pathways Gravelly Beach, Blackwall and Lanena

##### Health

- Doctors surgery
- Physiotherapist

##### Business / retail

- Agricultural / feed shop
- Bakery
- Banking
- Butcher
- Cafes
- Clothing
- Concrete and landscaping supplies
- Grocery
- Hairdressers
- Hardware
- Hotel
- Mechanic
- Metal fabrication
- Pharmacy
- Post office
- Real estate agent
- Service stations

### 3.1.3 Employment

According to the 2021 Census<sup>4</sup> a high proportion (49%), of residents in the Structure Plan Area are not in the labour force. This is either because they are under 15 years of age (15%) or have nominated that they are not working or looking for work. Almost 50% of residents not in the labour force are 60 years of age or over. This corresponds with the age profile of the community discussed earlier. Only 2% of residents responded that they were unemployed at the time of the census.

98% of employed persons worked in Tasmania with the remaining working in other Australian states. Most of those employed in Tasmania worked in Northern Tasmania (91%) with small numbers working in Hobart or North West Tasmania. 49% of people working in Northern Tasmania worked in the City of Launceston local government area with 38% in the West Tamar municipality. Notably 24% worked in the Grindelwald / Lanena locality (which includes the structure plan area) and a further 10% in either Legana or Riverside indicating a relatively high proportion of people who work locally or only a short distance away.<sup>5</sup>

Most people who went to work on census day 2021 travelled to work by private vehicle (84%). Nobody used public transport to go to work and only 4% used active transport (walking or cycling). On census day in 2021, 62 people worked from home, more than twice as many people as used active transport. On the 2016 Census day, only 27 people indicated they worked from home.<sup>6</sup>

The reliance on private motor vehicles for transport is reflected through the car ownership rates. In 2021, 98% of households had at least one car and 66% of households had two or more vehicles.<sup>7</sup>

The current and historical zoning of Exeter has included large areas in the Local Business Zone that are being used for community purposes or housing which may distort the perception that there is an adequate supply of commercial land.

Previous rounds of consultation have not identified the need for additional land for business activity however the range or type of business was raised. The strategic location of Exeter on an alternative travel route from North West Tasmania to Bell Bay via Frankford Road may be an opportunity for a transport or distribution centre in the vicinity of Exeter. Further investigations into the economic feasibility and the capacity of the transport network is required to confirm this.

#### *Planning response*

- *Allocation of residential land should provide for future population growth.*
- *Housing should be adaptable and provide opportunities to age in place and potentially down size.*
- *There may be demand for additional community services that are accessible and meet the needs of an ageing population.*
- *Local employment opportunities are facilitated through existing capacity in the local business and industry zones.*

<sup>4</sup> Australian Bureau of Statistics 2021 Census – employment, income and education – Labour Force Status by Age in Five Year Groups

<sup>5</sup> Australian Bureau of Statistics 2021 Census – employment, income and education – Place of Work

<sup>6</sup> Australian Bureau of Statistics 2016 and 2021 Census, method of travel to work (Employed persons aged 15 years and over)

<sup>7</sup> Australian Bureau of Statistics 2021 Census – selected dwelling characteristics – number of motor vehicles. Note the percentages were calculated excluding where the number of vehicles was not stated or not applicable.

## 4 Residential land supply and growth

The previous section has provided a good understanding of the community and how it has grown and changed over time.

This section will examine the current capacity of the Structure Plan Area to accommodate anticipated demand for housing and whether that is sufficient land to accommodate growth for the next 20 years.

Existing supply is based on the current Planning Scheme Zoning.

Figure 14 is an extract of the West Tamar Local Provisions Schedule zoning at December 2022.

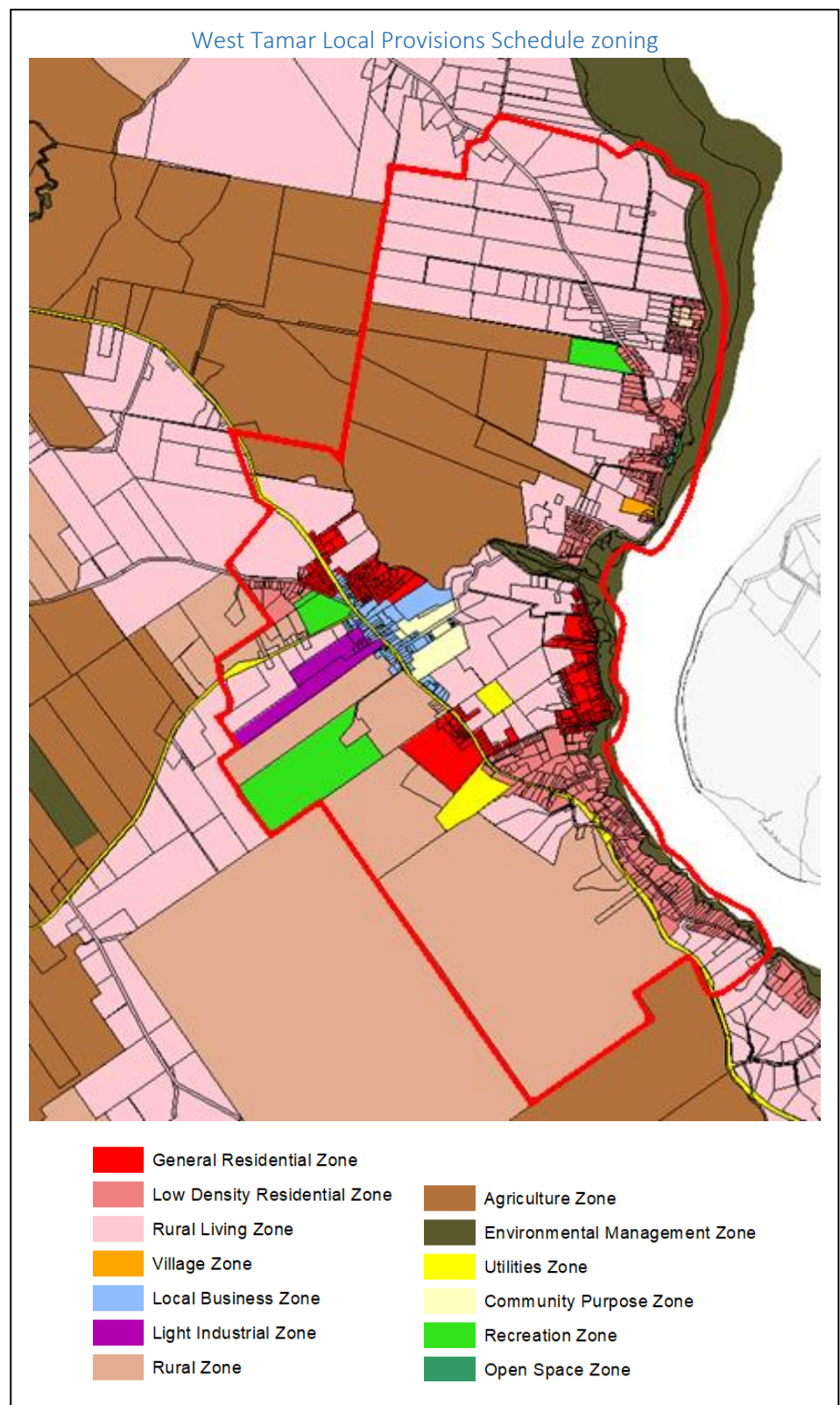


Figure 14: Zones under the West Tamar Local Provisions Schedule 2022

## 4.1 Existing land supply for residential growth

Existing supply of residential land includes:

- **Broadhectare supply** – sites zoned for residential development, with capacity for subdivision into 10 or more lots. Properties may have an existing dwelling or infrastructure on site.
- **Infill supply** is from a lot/dwelling construction perspective, residential development occurring within the established residential area (not on broadhectare sites) that yield less than 10 dwellings per site.

It considers the existing zoning only. Data was extracted from Council's rates information to identify vacant lots and lots capable of development by land area.

### 4.1.3 Broadhectare supply

The capacity of broadhectare land to accommodate growth has been calculated as a likely practical yield using the below methodology:

- 80% developable with 20% for roads/parks which means 8000m<sup>2</sup> per hectare is available for residential lots;
- Constrained land where the limitations on development are known was excluded from the land available for development (eg threatened vegetation);
- Where existing dwellings were present on the site, allowance was made to retain the dwelling and a reasonable curtilage; and
- Lot sizes and density were calculated based on consistency with the surrounding area and recently released lots as a likely market deliverable, for example in Exeter the lot sizes developed in the General Residential Zone are around 700m<sup>2</sup> rather than the 450m<sup>2</sup> minimum lot size included in the planning scheme.

Four sites have been identified as providing capacity for broadhectare development as shown in Figure 15.

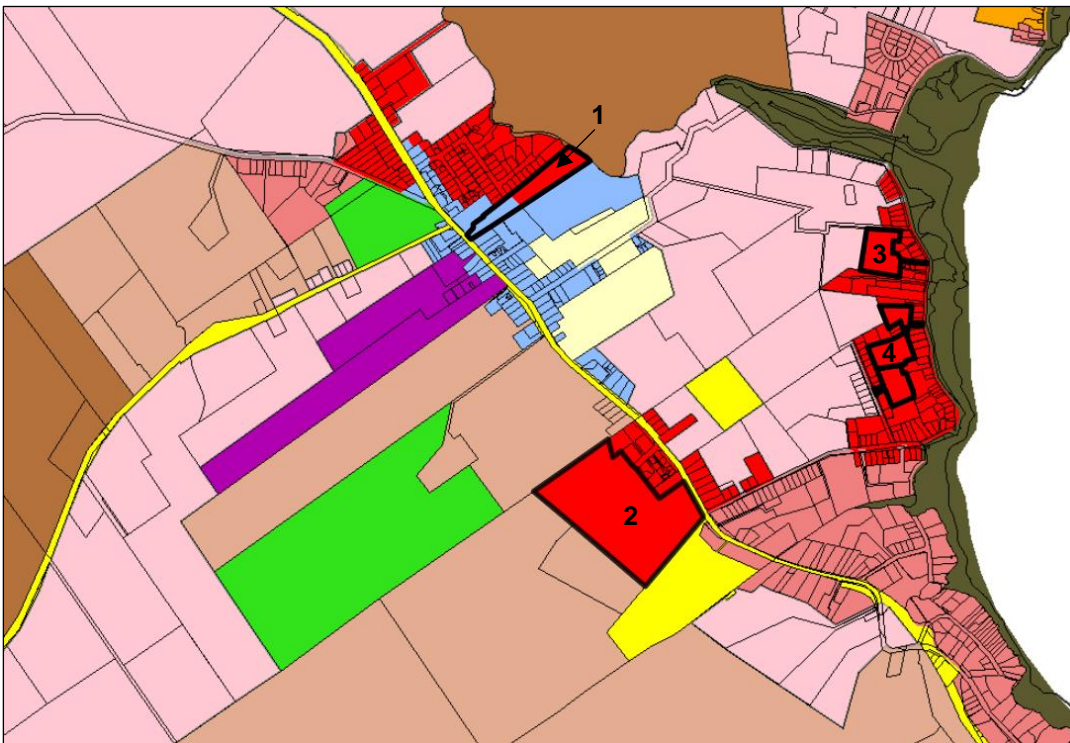


Figure 15: Locations of sites for Broadhectare supply

Table 5 provides a summary of the number of lots that could practically be developed on broadhectare land in the Structure Plan area based on the above methodology.

*Table 5: Potential broadhectare residential land supply*

Sites	Area (ha)	Number of lots	Timeframe for development
			Short term (0-5 years) Medium (5 to 10 years) Long term (10+ years)
Area 1	2.57ha (~8140m <sup>2</sup> in Local Business Zone)	20	<b>Short term</b> Indications that an application for development will be made in the short term. Take up of the Wildmore Crescent subdivision indicates high demand in this location.
Area 2	12.83ha (~1.95ha constrained)	40	<b>Short to Medium term</b> Indications that the development of this site is likely in the short to medium term but subject to developer intent.
Area 3	1.7ha	17	<b>Medium term</b> Existing dwelling and infrastructure requirements (new road)
Area 4	2.83ha	24	<b>Medium to long term</b> 3 landowners would ideally need to collaborate for development and restrictions in relation to existing dwellings and access.
<b>Total</b>		<b>91</b>	

Combined, the areas could produce approximately 91 lots, however only 20 to 60 of these lots are anticipated to be developed in the next 5 years with the remaining lots in the medium to long term.

#### 4.1.2 Infill Supply

Infill is another source of residential supply however the timing of supply can be harder to predict as there are multiple landowners with their own interests in when or if development will occur.

Opportunity for infill development was calculated based on:

- All vacant properties with a residential zoning (General Residential, Low Density Residential or Rural Living Zones);
- Properties over 1000m<sup>2</sup> in the General Residential Zone with an existing single dwelling. Dwelling capacity was calculated as one dwelling per 400m<sup>2</sup> (based on the median densities achieved for multiple dwellings in the locality) or the approved density where there is a planning permit in place;
- Properties over 1ha in the Low Density Residential Zone and therefore capable of subdivision with supply calculated as one dwelling per 5000m<sup>2</sup> or one dwelling per lot where the existing lot size is less than 1ha;
- Rural Living if the lot size was twice the minimum lot size or more however none were present;
- One dwelling per lot for vacant properties in the Rural Zone; and
- Local business zone only if there is an approved (but not yet completed) approval for a dwelling or multiple dwelling as the primary purpose of the Local Business Zone is for business uses.

Table 6 provides an assessment of the number of dwellings that could theoretically be delivered through infill development. However it is reiterated that infill development is difficult to predict and should not be relied on as a secure source of housing supply.

*Table 6: Potential residential land supply*

Location	Number of properties	Potential number of dwellings
Exeter	31	70*
Gravelly Beach	21	23
Blackwall	8	31
Lanena	13	13
<b>TOTAL</b>	<b>73</b>	<b>137</b>

\*includes 10 dwellings currently under construction

14 multiple dwelling applications for 31 dwellings were approved across the Structure Plan area in the 10 years since between 2011 and 2021. One multiple dwelling application has been approved in 2022 for two dwellings). The density of multiple dwelling approved varied however the mean (average) density was one unit per 651m<sup>2</sup> and the median was 1 unit per 488m<sup>2</sup>. The relevant planning scheme permitted a density per unit of one per 325m<sup>2</sup> and a minimum lot size of 450m<sup>2</sup> during this period.

Only two residential subdivisions were approved in the 10 years between 2011 and 2021, both on broad hectare sites, for 38 lots in Exeter and 13 lots in Gravelly Beach (of which only seven lots have been completed to date). Eight of the multiple dwelling applications (resulting in 17 dwellings) were approved within the 38 lot subdivision at Exeter.

Between 2011 and 2021, there were building approvals issued for 100 dwellings in the structure plan area. Multiple dwellings therefore made up 31% of housing supply over this period, albeit the majority within newly subdivided developments.

## 4.2 Demand for new housing

The Northern Tasmania Regional Planning Group are in the process of developing a Residential Demand and Supply Study. This work, which will include forecast population growth, is intended to be completed in the first half of 2023. The results of this study will be used to inform the anticipated demand for housing in the Structure Plan Area and will be incorporated into the Structure Plan when available.

### 4.2.1 Future growth

Anecdotally, and based on data around new dwellings, there is demand for new housing in the Structure Plan Area with the initial consultation, and representations to the West Tamar draft Local Provisions Schedule in 2021, identifying community support for opportunities for additional growth to be planned for in the area.

A number of sites have been identified for potential growth, in addition to the existing supply discussed above.

### 4.2.2 Between Exeter and Blackwall

Consultation about the Structure Plan and the draft Local Provisions Schedule identified strong interest from property owners for the area between Exeter township and Blackwall to be considered for residential development.

The area, circled in yellow in Figure 16, has been examined to determine suitability for residential development based on known information about the constraints of the land. Specific land suitability studies,

for example natural values assessments have not been completed at this time and would be expected to be undertaken as part of future development applications.

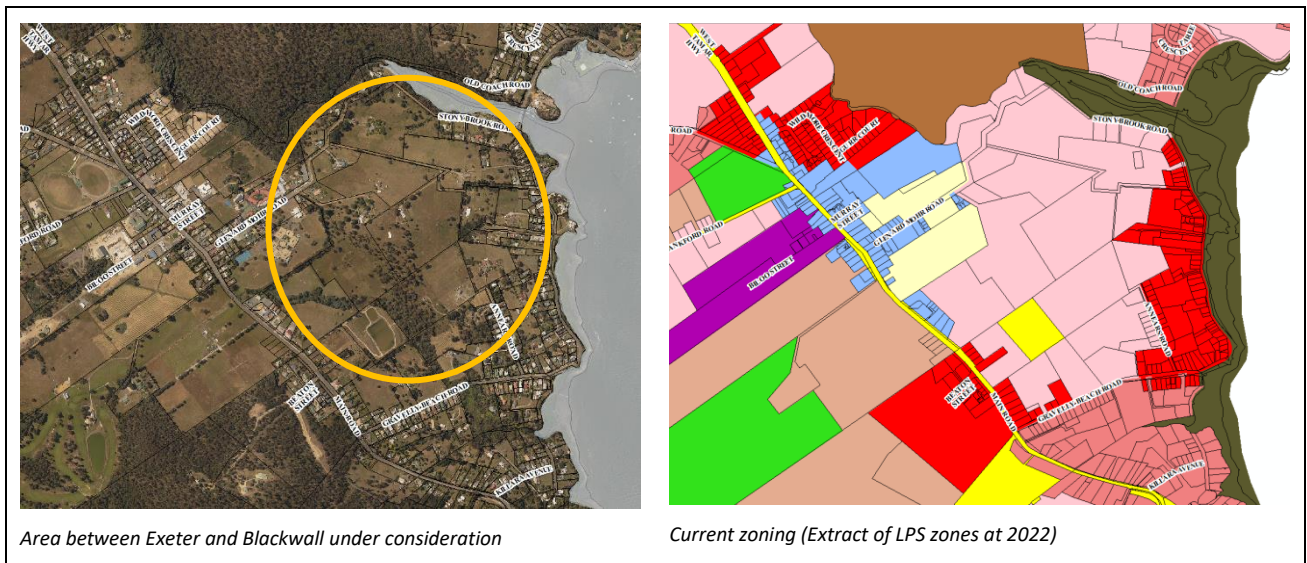


Figure 16: Area between Exeter and Blackwall considered for future growth and 2022 zoning

Figure 17 shows extracts of mapping showing the constraints and natural features in the area. The most significant constraint is the Sewerage Treatment Plant operated by TasWater which has an attenuation area under the Tasmanian Planning Scheme of 350m. This area was removed from consideration and influenced the consideration of the potential growth area boundary.

Based on the constraints and consideration of how the relevant overlay codes will continue to operate under the Tasmanian Planning Scheme, the area being considered was refined as shown in Figure 18.



Trall Road between Exeter and Blackwall

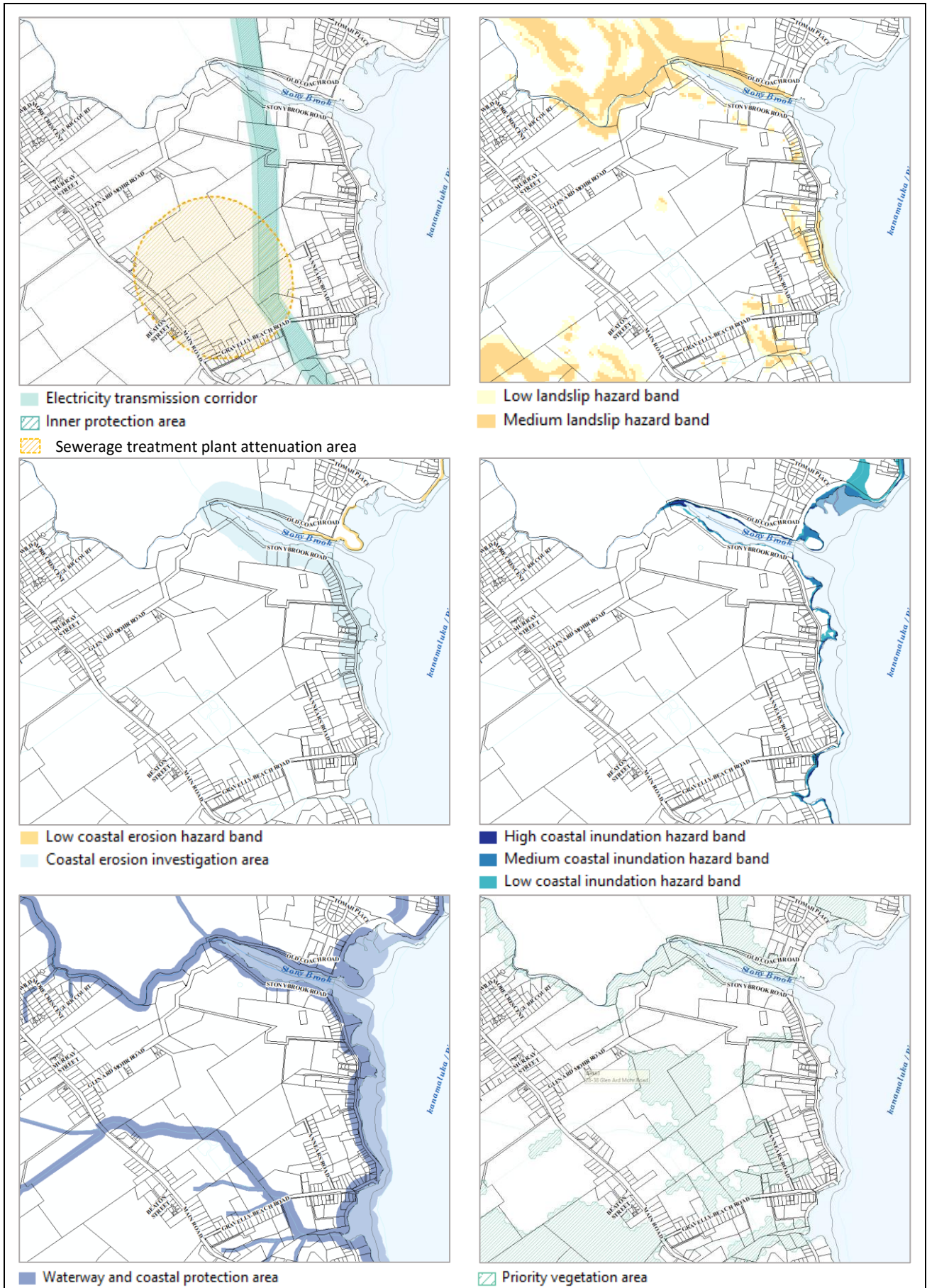
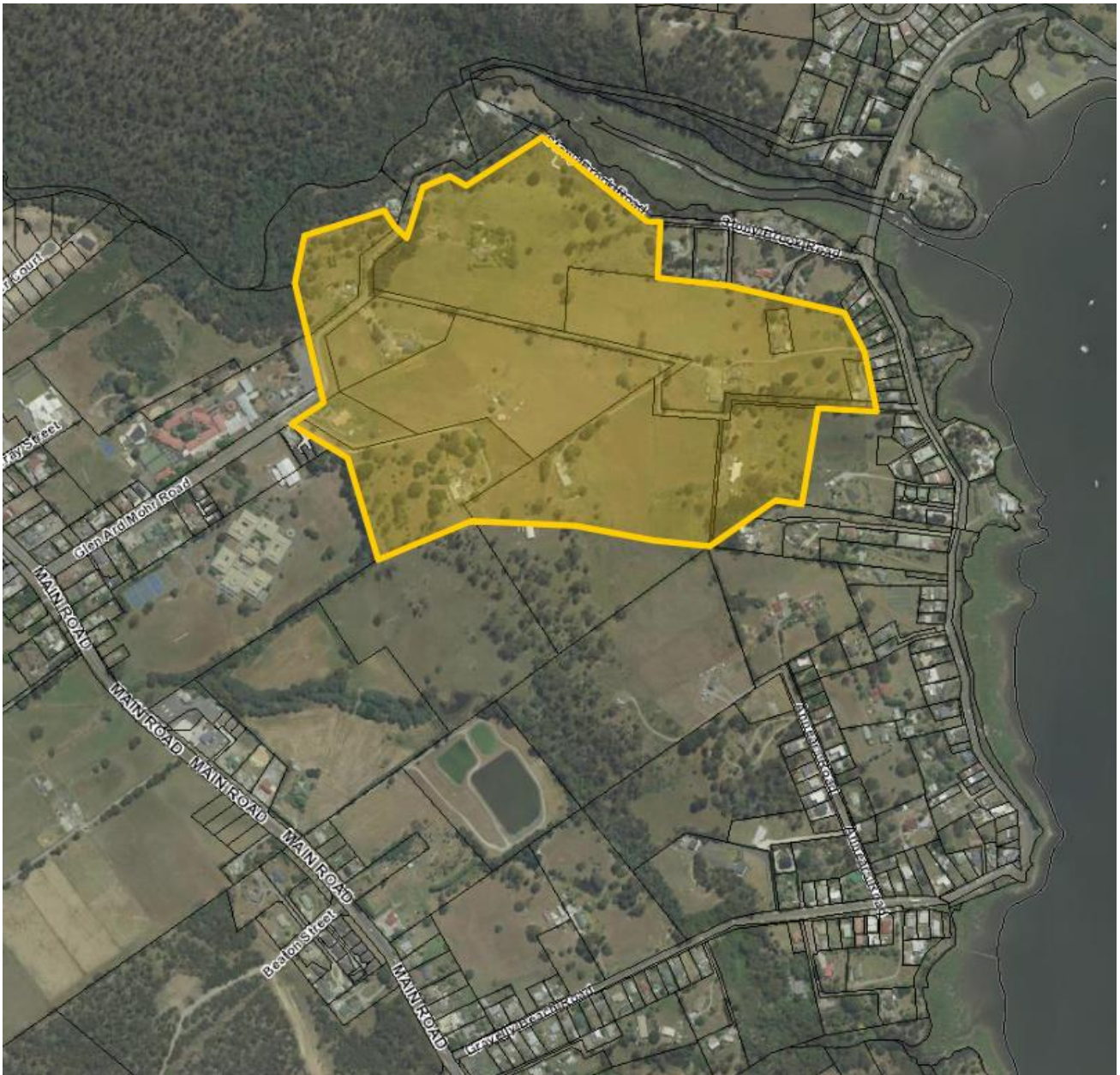


Figure 17: Exeter to Blackwall natural features and constraints



*Figure 18 - Refined Exeter to Blackwall growth area*

The 2014 Structure Plan identified an opportunity for development in this general area to facilitate a road connection between Gravelly Beach Road and Glen Ard Mohr Road. This road connection would provide an alternative link to the Exeter township and would also be required to facilitate development of the area and provide appropriate access to the potential residential lots. Council commissioned a traffic study to assess the impact of increasing residential densities between Exeter and Blackwall by considering:

- two growth scenarios with minimum lot sizes of 5000m<sup>2</sup> (81 additional lots) and 1ha (42 additional lots);
- the preferred alignment of a road that links Glen Ard Mohr Road with Gravelly Beach Road;
- the impacts of providing a road connection between Glen Ard Mohr Road and Gravelly Beach Road on the Gravelly Beach Road and Main Road junctions.

The Traffic Study concluded the use of Traill Road is the preferred route for the connection of Glen Ard Mohr Road and Gravelly Beach Road. It provides improved connectivity for future subdivision by providing a central road link. Road widening and minor realignment of the corridor will be required as part of the development of the road. Figure 19 shows the Traill Road connection and realignment requirements.



*Figure 19 - Glen Ard Mohr Road Connector Corridor Options<sup>8</sup>*

Modelling indicated that the Glen Ard Mohr / Main Road intersection will operate at an acceptable level until 2032 but deteriorates by 2042 which is less than 20 years away. A roundabout at this intersection would operate at a high level of service in both 2032 and 2042 and will cater for expected growth associated with the growth scenarios considered.

A T-junction was considered appropriate at the Traill Road – Gravelly Beach Road intersection (if developed) and would cater for the forecast traffic generation from the increase in residential density.

The majority of the area is within TasWater’s Water Serviced land – Figure 20. While there is no obligation for TasWater to provide additional connections the following matters would be considered in assessing whether a connection would be provided:

- Can be supplied with treated water;
- Are within 30 metres of our water reticulation main;
- Can receive the minimum flow and pressure at the connection point;
- Connection to the reticulation main would not cross land owned by a third party; and
- The physical characteristics or location of the property title do not require the application of unusual or unusually costly infrastructure, design, or installation techniques in order for the connection to be made.

While this indicates that a service connection to new lots is possible, Council will engage with TasWater to ascertain if there is capacity to provide water connections for the anticipated future development.

<sup>8</sup> Midson Traffic Pty Ltd, *Exeter Traffic Study – Options Analysis*, October 2022

Part of the area is included in TasWater’s Sewer Serviced Land – Figure 20. Preliminary discussions with TasWater have indicated potential capacity issues in increasing the number of serviced lots. Larger lots, which is in keeping with the submissions Council has previously received regarding future development in this area, should be able to accommodate on-site wastewater treatment and disposal without the need to connect to reticulated sewerage.

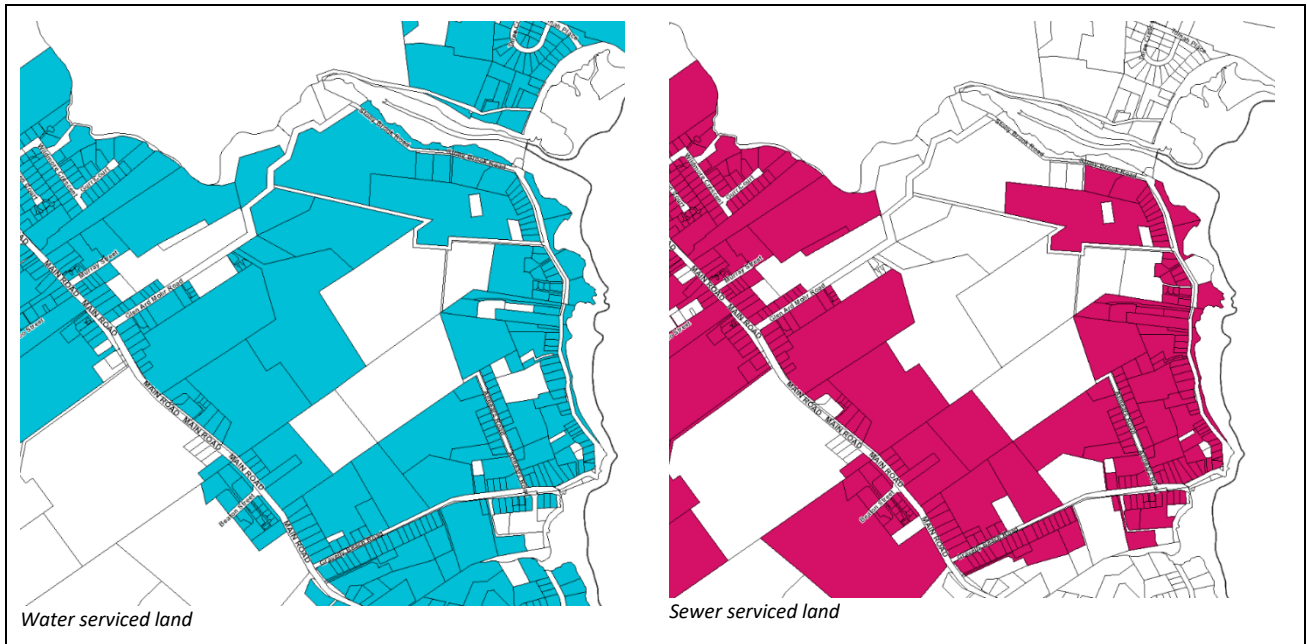


Figure 20: Exeter to Blackwall water and sewer serviced land (TasWater)

Development of this potential growth area:

- provides an opportunity to provide a stronger connection between Exeter and Gravelly Beach;
- Avoids the Sewerage Treatment Plant attenuation area and constraints that cannot be managed through an assessment against the Tasmanian Planning Scheme;
- provides a break in the urban area between the 2 areas with larger lifestyle lots; and
- does not require significant infrastructure upgrades, noting that the construction of Traill Road and extension of services would be required as part of the development.

It is proposed that the area be identified as a Growth Area in the Structure Plan. An amendment to the West Tamar Local Provisions Schedule will be required to change the zone to the Low Density Residential Zone to facilitate development with a minimum lot size of 5000m<sup>2</sup>. This will be subject to the approval of the Tasmanian Planning Commission.

### 4.2.3 Areas for future investigation

Two other sites have been identified for potential growth as shown in Figure 21.



*Figure 21 - Areas for further investigation*

**Area A – Exeter North** — Area A may provide an extension to the General Residential area subject to availability of sewerage infrastructure and appropriate access. There is potential for approximately 60 lots subject to consideration of demand, constraints and infrastructure provision.

**Area B – Gravelly Beach** — Area B, accessible via Tomah Place, may provide an extension to the Low Density Residential area to the east. Demand, infrastructure provision and constraints require further investigation prior to enabling development. The preferred minimum lot size is 5000m<sup>2</sup>.

Both sites appear to have minimal natural features and constraints based on the code overlays from the Local Provisions Schedule and would likely be able to be managed through the planning assessment processes – Figure 22.

Demonstrated demand for more residential land and capacity of infrastructure services are likely to be determining factors for the timing and density of future development.

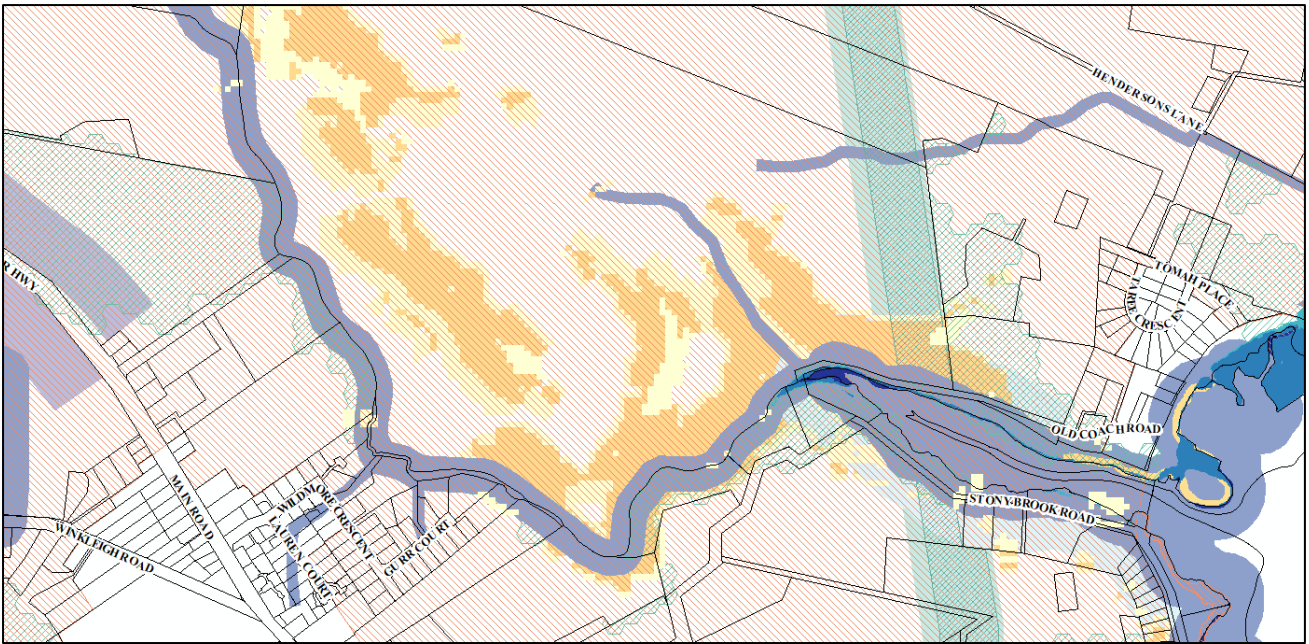


Figure 22: Areas for further Investigations natural features and constraints

*Planning response*

- Identify the area between Exeter and Blackwall for future low density residential growth.
- Identify the Exeter North and Tomah Place Gravelly Beach sites as investigation areas to potentially accommodate additional residential growth.
- Review the timing and provision of additional land for growth when the Northern Tasmanian Residential Demand and Supply Study is complete.



## 5 Transport

How people move around and access the area is important both for safety and efficiency of the transport network and its contribution to liveability.

### 5.1 Roads

The Structure Plan area is serviced by a network of roads with key roads being:

- **Main Road** – to access the Exeter business area and provide a key north south road corridor linking the town with Launceston to the south and Beaconsfield and townships to the north. It also provides a freight link to Bell Bay Port via the Batman Highway;
- **Frankford Road** – providing an important freight link between Devonport and Bell Bay Port. It also provides a direct connection for tourists disembarking the Spirit of Tasmania to the region;
- **Gravelly Beach Road and Rosevears Drive** – providing connections to the coastal communities and a scenic coastal drive.

Previous traffic studies were completed in 2015 and 2019.

As part of the review of the 2014 Structure Plan, Council identified a number of traffic impacts that required further analysis:

- An assessment of the impact of increasing residential densities between Exeter and Blackwall – which is further discussed in section 4;
- An assessment of a new road that provides a connection to Murray Street from the Frankford Road/ Main Road junction with a link to Wildmore Crescent; and
- Provision of recommendations based on the above for treatments (if required) in Main Road at the Glen Ard Mohr Road and Frankford Road junctions.

#### 2015 Exeter Traffic Study

A report titled 'Exeter Traffic Study' was prepared by Midson Traffic in 2015. The study investigated the various recommendations of the Exeter Structure Plan and tested their implications on traffic flow and road safety. The findings of this report supported the recommendations of the Structure Plan and provided additional detail for implementation over the next twenty years.

The key recommendations of the report were as follows:

- Two roundabouts were proposed. These were located at the Main Road/ Glen Ard Mohr Road junction and the junction of Main Road/ Frankford Road.
- A link road was recommended between Glen Ard Mohr Road and Gravelly Beach Road.
- A link road was recommended between Murray Street and Main Road/ Frankford Road.
- Improved pedestrian crossing facilities along the Main Road corridor.
- Implementation of a new park at the corner of Biloo Street and Main Road.

#### 2019 Exeter Traffic Study Options Report

GHD prepared a study titled "Exeter Traffic Study – Options Report" in October 2019. The aim of the study was to review potential options and improvements in Exeter to manage conflicts between the various road users with competing priorities.

The report concluded that the following options be considered for further design and investigation:

- Installation of roundabouts at the Main Road/ Frankford Road and Main Road/ Glen Ard Mohr Road junctions.
- Installation of entry thresholds in Main Road near Frankford Road and Glen Ard Mohr Road.
- Installation of additional kerb outstand crossing treatments throughout Main Road.
- Formalisation of on-street parking arrangements.

*Source: Midson Traffic Pty Ltd, Exeter Traffic Study – Options Analysis, October 2022*

Midson Traffic completed the *Exeter Traffic Study – Options Analysis* in October 2022 which addresses the above issues and makes the following conclusions:

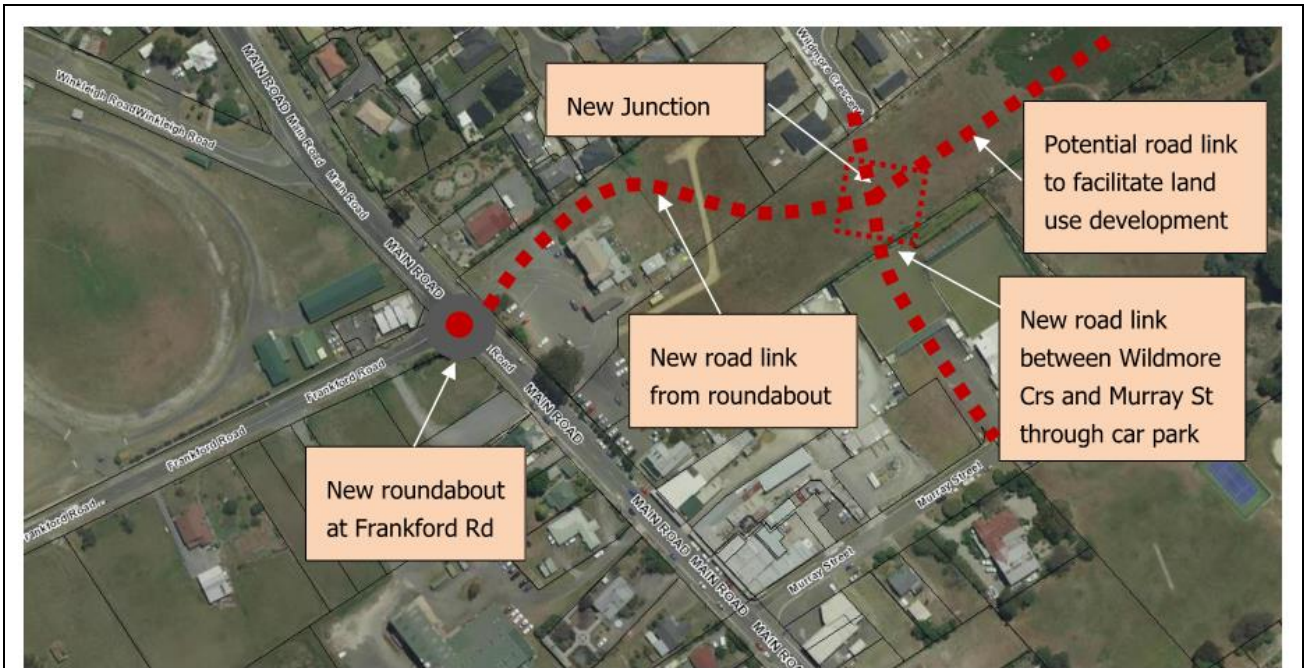
- Traill Road is the preferred alignment for the **link between Glen Ard Mohr Road and Gravelly Beach Road** (Figure 19);
- There are two options for providing a link between **Murray Street to Wildmore Crescent and to the Main Road** either at the Frankford Road intersection or a new junction north of 170 Main Road, both utilising the existing carpark associated with the Hotel (Figure 23). The link road will also enable vehicle circulation around the business areas and provide better access to parking areas.

The design of the new link road would need to carefully consider the revised access to the hotel, changes in car parking arrangements and additional parking opportunities along the length of the road. The link through to Murray Street will also require consideration of the potential loss / relocation of a bowling green and the overall community benefits;

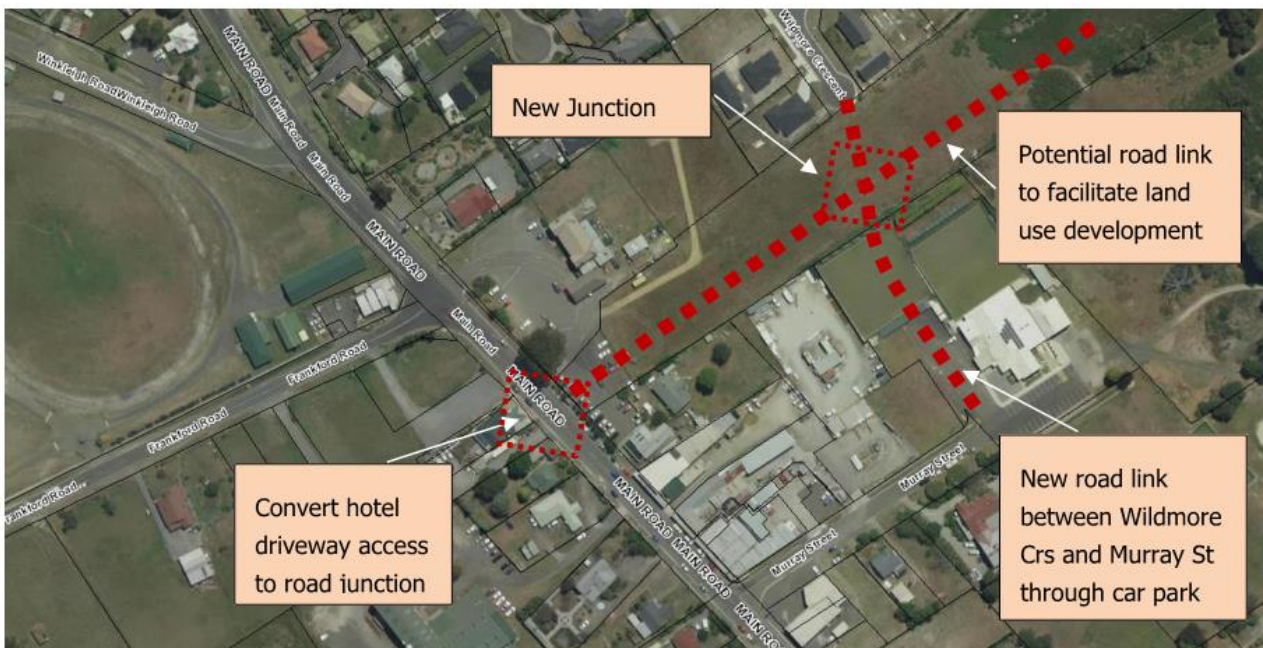
- A roundabout is strongly recommended at the **Main Road/Frankford Road junction** to facilitate the Murray Street connector link road, improve traffic performance and improve road safety. Note that the Department of State Growth have indicated significant land acquisition may be required to facilitate these works and exiting bus stops may require relocation;
- A roundabout at the **Main Road/Glen Ard Mohr Road junction** is recommended to facilitate future traffic growth associated with development within Exeter; and
- The **Main Road/Murray Street junction** is performing at an acceptable level and can accommodate forecast traffic flows.



*Main Road, Exeter*



*Indicative Murray Street to Wildmore Crescent link via Frankford Road junction*



*Indicative Murray Street to Wildmore Crescent link via Main Road junction*

Source: Midson Traffic Pty Ltd, Exeter Traffic Study – Options Analysis, October 2022

*Figure 23: Options for links between Murray Street, Wildmore Crescent and Main Road*

Car parking has been raised through consultation as a constraint for the business area. Further investigations will be required to properly consider where and when any future car parking is provided however:

- There is an opportunity for additional car parking to be provided behind the library on Main Street and near the Community Hub on Murray Street; and
- The recommended road link between Murray Street, Wildmore Crescent and Main Road will facilitate better access to off-street car parking associated with the Main Road business area and provide additional capacity for on-street parking.

## 5.2 Active transport

Active transport includes walking and cycling and doesn't rely on motor vehicles. Active transport can be used for recreation purposes in addition to accessing work and services.

Only 4% of the working population in the Structure Plan Area travelled to work by walking or cycling on census day in 2021, however it is acknowledged that there are a relatively low number of jobs in the area and the majority of people travel outside the Structure Plan Area for work. Anecdotally, it is reported that walking is a popular exercise and recreational activity in the Structure Plan Area and to access the riverside parkland and pathways.

Pedestrian safety and improved pedestrian pathways was a strong theme from consultation with the community with specific reference to:

- Safe crossing points on Main Road at the main business area and at the Exeter High School farm;
- Improvements to the paths along Main Road and particularly between the retirement village and the business area (noting that some improvements to the footpath in this location have recently been completed); and
- Improvements to the Goat Track between Murray Street and Gravelly Beach Road via Stony Brook Road; and
- Improvements to pathways within the residential areas.

Figure 24 shows the existing formal pathway network (eg concrete or bitumen seal) and indicates where there are opportunities to improve connections and safe crossings.

Council has recently prepared the West Tamar Trails Strategy that seeks to provide a strategic framework to inform future funding to deliver a comprehensive trail network that is accessible for all, promotes active living and encourages tourism. The Strategy recommends improvements in the Structure Plan area for the:

- Gravelly Beach Trail; and
- Exeter to Gravelly Beach Trail.

Appendix 1 includes an extract from the Trails Strategy relating to the recommended trail improvements. Implementation of the Strategy will be subject to future funding and a timeframe for this is not known at this time.



Figure 24: Pathway network

### 5.3 Public transport

Bus Stops are located on Main Road (south of Frankford) and on Gravelly Beach Road.

Manions Coaches operates a general access bus service with stops in Exeter. It provides 9 services a day on weekdays between Beauty Point and Launceston (with one of the services only operating on school days) and 3 services a day on weekends and public holidays. The bus timetable suggests the journey to Launceston takes around 50 minutes.

Responses to the 2021 Census indicated that nobody travelled to work by public transport on census day. There may be an opportunity for additional trips via bus if the frequency and timing of the services were more suitable for workers. However, with a variety of workplaces to accommodate this level of service may not be feasible.

#### *Planning response*

- *A link road between Glen Ard Mohr Road and Gravelly Beach Road should generally align with Trill Road and be required as part of the future development of the area to accommodate residential growth.*
- *A road and pedestrian connection should be provided from Murray Street, to Wildmore Crescent and Main Road to facilitate efficient and safe vehicle and pedestrian movement and facilitate better access to parking in the Exeter business area.*
- *Plan for potential future roundabouts at the Main Road/Frankford Road and Main Road/Glen Ard Mohr Road junctions.*
- *Accessible and safe transport solutions (including walking, cycling and public transport) should be further investigated and a strategy implemented to support improved access to local services and reduce trips by private car.*



*Bus Stop in Exeter*

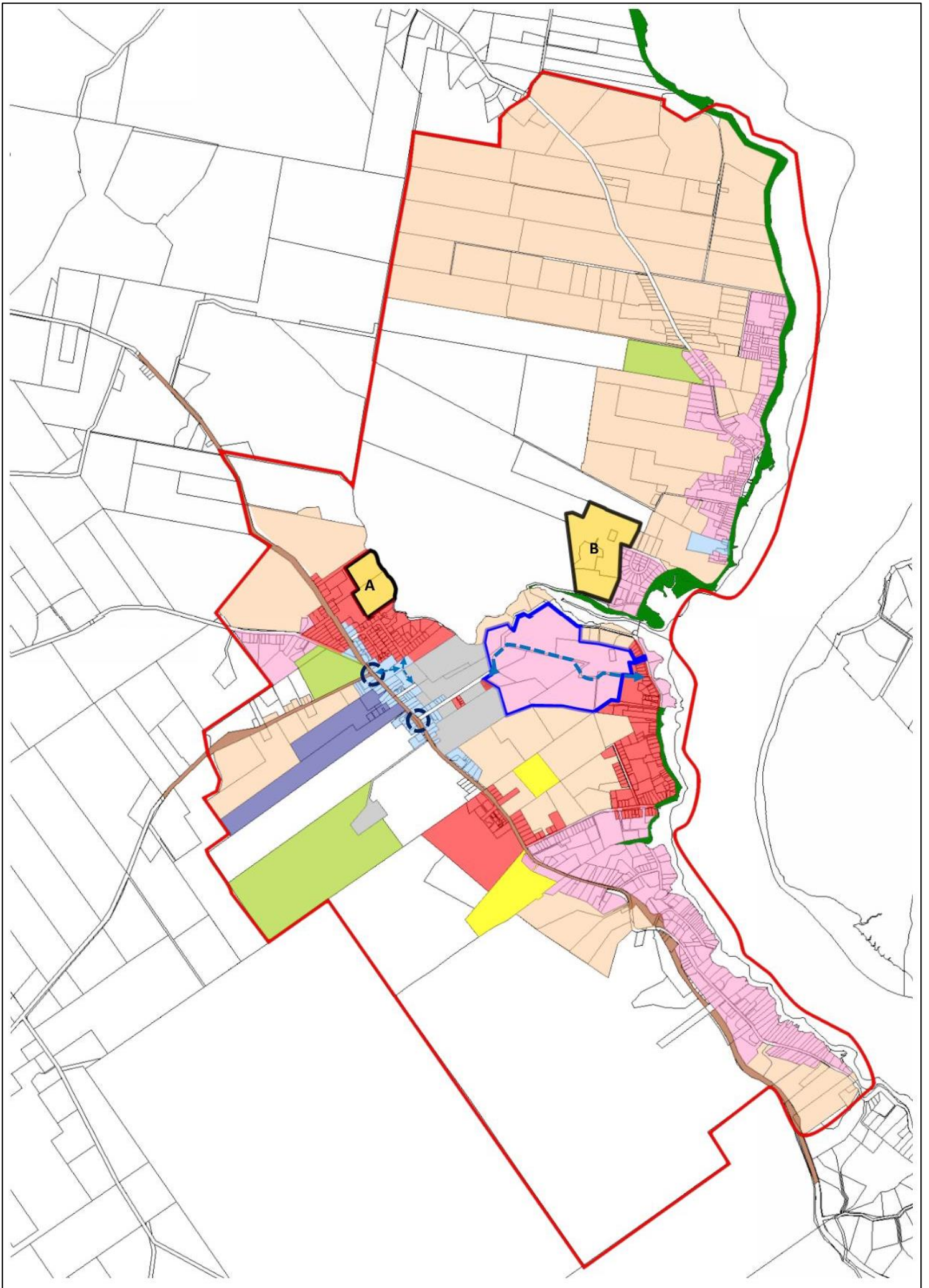
## 6 Draft Structure Plan

The draft Structure Plan divides the study area into precincts that represent the intended future land use.

Section 6.2 details the planning requirements for future use and development in addition to the standard requirements of the Tasmanian Planning Scheme. Amendments to the West Tamar Local Provisions Schedule will be required to enable to future development opportunities proposed by the Structure Plan and detailed in section 6.2.

### 6.1 Structure Plan Precincts and Intent

	<b>General Residential</b> Residential development from single dwellings to multiple dwellings where a full range of services are available.
	<b>Low Density Residential</b> Single dwellings on larger lots >5000m <sup>2</sup> , generally with some services.
	<b>Residential growth area</b> A low density residential growth area for single dwellings on larger lots >5000m <sup>2</sup> , generally with some services. Development is expected in the short to medium term
	<b>Investigation Areas</b> <ul style="list-style-type: none"><li>• Investigation Area A</li><li>• Investigation Area B</li></ul>
	<b>Rural Living</b> Single dwellings on large lots generally with rural characteristics.
	<b>Local Business</b> Local businesses and community services
	<b>Light Industry</b> To provide for light industrial uses where off site impacts are minimal or can be managed
	<b>Community Purpose</b> Community and education facilities.
	<b>Recreation</b> Sports and recreation facilities servicing the local community and the region.
	<b>Utilities</b> TasWater facilities
	<b>Rural</b> Balance area for rural and agricultural activities
	<b>New road connections</b>
	<b>New intersection treatment</b>
	<b>Coastal open space</b>
	<b>State Roads</b>
	<b>Structure Plan area boundary</b>



## 6.2 Requirements for use and development

The following requirements are intended to inform future amendments to the West Tamar Local Provisions Schedule and the conditions which future use and development are intended to meet. Where no specific requirements are stated, the existing provisions of the Tasmanian Planning Scheme would apply.

### 6.2.1 Residential Growth Area (between Exeter and Blackwall)

- a. A low density residential growth area for single dwellings on larger lots, generally with some services.
- b. Minimum lot size of 5000m<sup>2</sup>.
- c. Not suitable for multiple dwellings.
- d. A road connection is to be provided between Glen Ard Mohr Road and Gravelly Beach Road generally in accordance with the alignment shown in Figure 25.
- e. All relevant codes in the Tasmanian Planning Scheme to apply to future use and development.



Figure 25: Road connection between Glen Ard Mohr Road and Gravelly Beach Road

### 6.2.2 Investigation Area A

- a. Further investigations into demand, density, site suitability, serviceability, access and traffic impact are required.
- b. This site could provide an opportunity for expansion of the retirement village or for aged care.
- c. Preferred use and development is consistent with the General Residential Zone.

### 6.2.3 Investigation Area B

- a. Further investigation into demand, density, site suitability, serviceability, access and traffic impact are required.
- b. Preferred use and development consistent with the Low Density Residential Zone with lot sizes 5000m<sup>2</sup> and over.

### 6.2.4 Infrastructure Requirements

- a. Glen Ard Mohr Road to Gravelly Beach Road link:
  - Aligned generally in accordance with Figure 25 along Traill Road;
  - Corridor width of 18 meters and a sealed road width of 8.9 meters;
  - T-Junction at Gravelly Beach Road; and
  - A sealed footpath extending the length of the road.
- b. Murray Street, Wildmore Crescent and Main Road link:
  - The preferred connection to Main Road is at the Main Road/Frankford Road junction as a fourth leg to the future roundabout (Figure 26);
  - Corridor width of 18 meters and a sealed road width of 11 meters;
  - Capacity for on-street car parking along the length of the new road;
  - Revised access arrangements for the hotel;
  - A sealed footpath extending the length of the road; and
  - T-junction or roundabout at the new junction connecting Murray Street, Wildmore Crescent and Main Road.

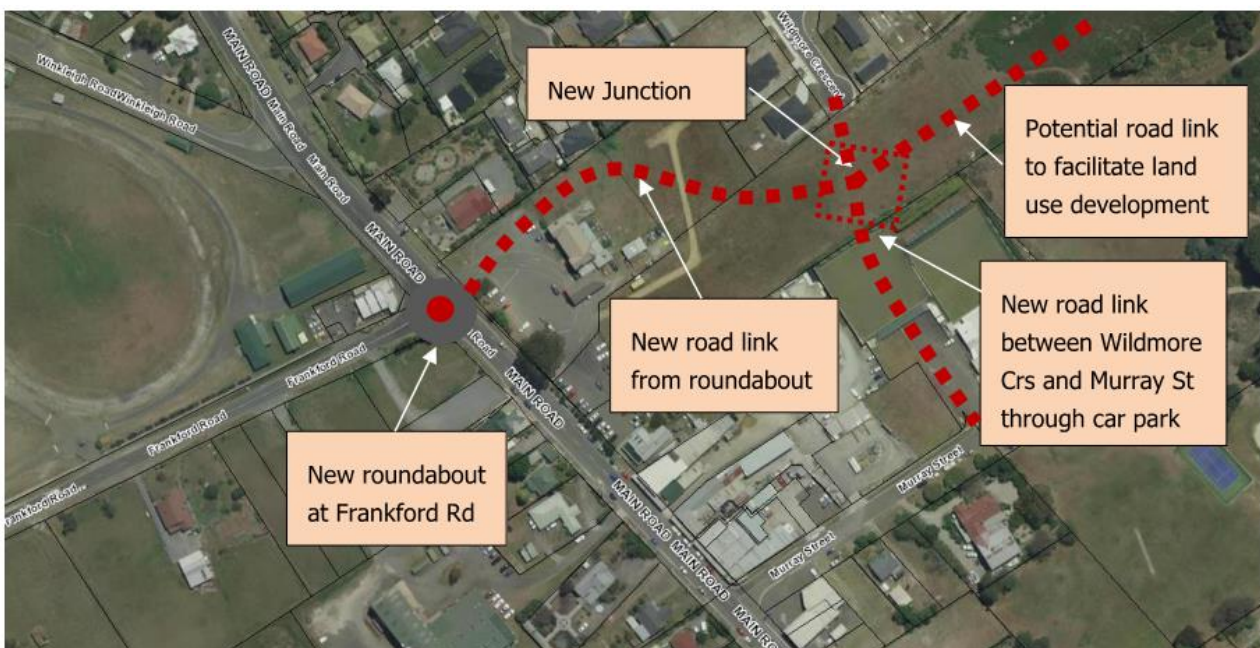


Figure 26: Indicative Murray Street to Wildmore Crescent link via Frankford Road junction

- c. Main Road/Frankford Road junction:
  - A roundabout at the junction to improve traffic performance and improve road safety.
- d. Main Road/Glen Ard Mohr Road junction:
  - A roundabout at the junction to improve traffic efficiency and cater for future growth.
- e. Pedestrian and cycle pathway connections:
  - New residential development provides dedicated pedestrian paths;
  - Improved connections as shown on Figure 24 are facilitated; and
  - Safe crossing points of Main Street are facilitated.



*Gravelly Beach*

## 7 Implementing the Structure Plan

Following the consultation period, Council will consider all the representations that are received and make any necessary revisions before adopting the final Exeter and District Structure Plan.

### 7.1 Recommended amendments to the West Tamar Local Provisions Schedule (LPS)

To effectively implement the Structure Plan amendments to the West Tamar Local Provisions Schedule will be required. The recommended amendments are listed below with an indication of the amendments that Council will seek to progress and those that are recommended to be progressed by landowners.

The timing of the amendments will be subject to available resources and the approval of the Tasmanian Planning Commission.

Amendments to the West Tamar Local Provisions Schedule	Recommended to be progressed by:
1. Amend the zone of the future growth area between Exeter and Blackwall to the Low Density Residential Zone and include in the Residential Density and Supply Specific Area Plan.	Council
2. Consequential zoning amendments to: <ul style="list-style-type: none"> <li>• Include the Community Hub in the Community Purposes Zone;</li> <li>• Child care centre on Glen Ard Mohr Road in the Community Purposes Zone;</li> <li>• 20-24, 26 and 40 Glen Ard Mohr Road in the General Residential Zone;</li> <li>• Properties along Stony Brook Road included in the Rural Living Precinct to change to the Rural Living Zone A (currently Rural Living Zone C);</li> <li>• Include the small serviced lots on the western side of Annears Road in the General Residential Zone; and</li> <li>• Include the developed part of 144 Main Road, Exeter (retirement village) in the General Residential Zone to reflect existing land use.</li> </ul>	Council
3. Environmental Management Zone amendments currently under consideration by the Tasmanian Planning Commission: <ul style="list-style-type: none"> <li>• 14 and 15 Stony Brook Road to the Rural Living Zone A;</li> <li>• 82A Gravelly Beach Road to the General Residential Zone;</li> <li>• 162, 164, 180 and 188 Gravelly Beach Road to the Low Density Residential Zone subject to the Residential Density and Supply Specific Area Plan; and</li> <li>• 267 Gravelly Beach Road to the Low Density Residential Zone.</li> </ul>	Council
4. Include a road network plan / specific area plan that specifies the requirements for the: <ul style="list-style-type: none"> <li>• Link road between Glen Ard Mohr Road and Gravelly Beach Road;</li> <li>• Link road between Murray Street, Wildmore Crescent and Main Road;</li> <li>• Roundabouts at the Main Road/Frankford Road junction and the Main Road/Glen Ard Mohr Road junction.</li> </ul> <p><i>Note: the best options to facilitate these requirements will require additional consideration, including options such as development contributions.</i></p>	Council
5. Subject to the outcomes of the investigations, amend the zones of the Investigations Areas to facilitate future development.	Landowner

## Appendix 1 - Supporting Maps

## Natural Assets and Hazards



### Coastal Erosion Hazard bands

- High coastal erosion hazard band
- Medium coastal erosion hazard band
- Low coastal erosion hazard band
- Coastal erosion investigation area

### Priority Vegetation Area

- Priority vegetation area

### Coastal Inundation Hazard bands

- High coastal inundation hazard band
- Medium coastal inundation hazard band
- Low coastal inundation hazard band
- Coastal inundation investigation area

### Waterways and Coastal Protection Area

- Waterway and coastal protection area

### Landslip Hazard bands

- Low landslip hazard band
- Medium landslip hazard band
- Medium-active landslip hazard band
- High landslip hazard band

## Underground services



### Sewer

- Manhole
- Sewer
- - - House connection
- Rising main

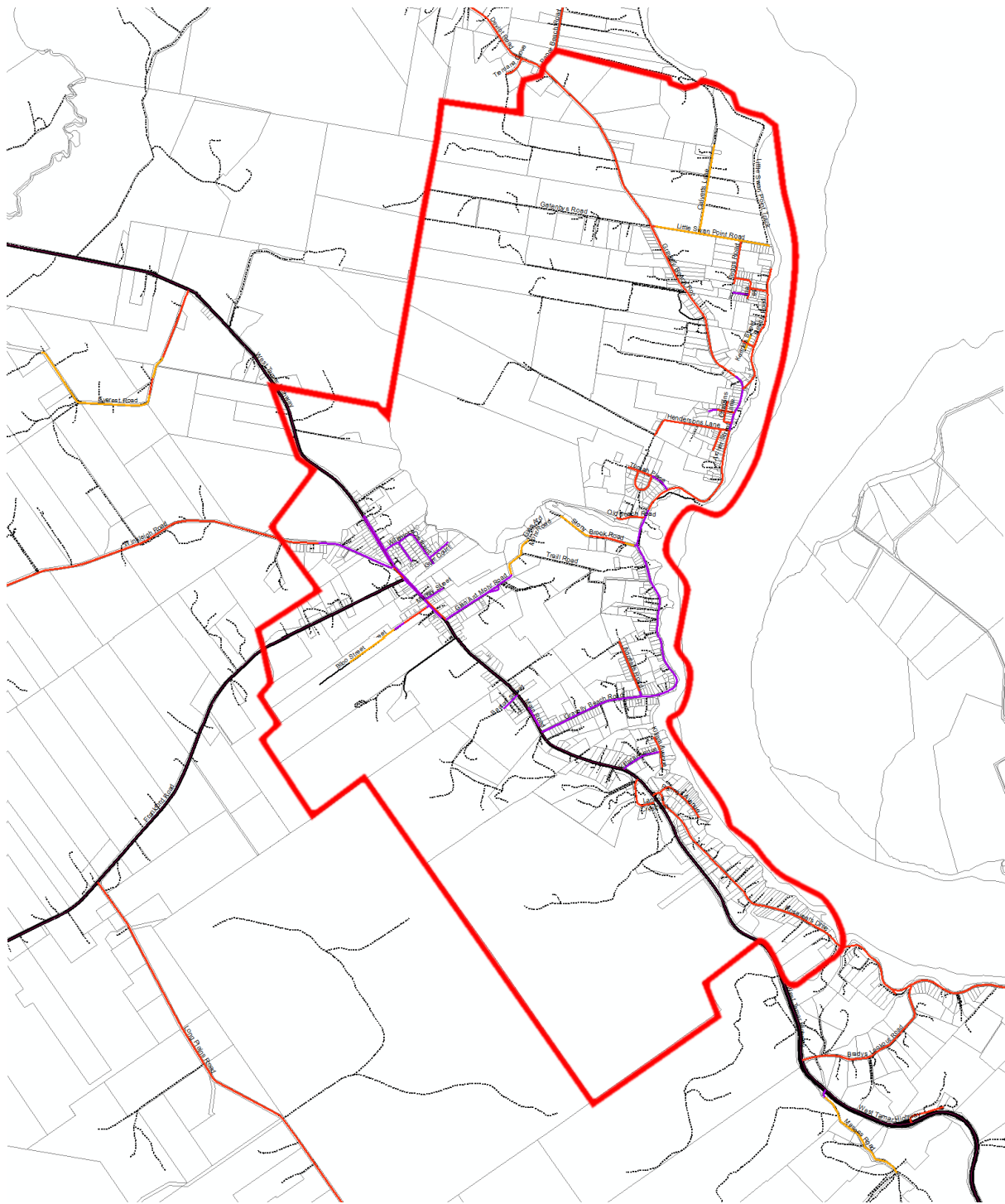
### Water

- Water Supply
- - - Water lateral

### Stormwater

- Pit
- Gravity line
- Open drain line

## Road types



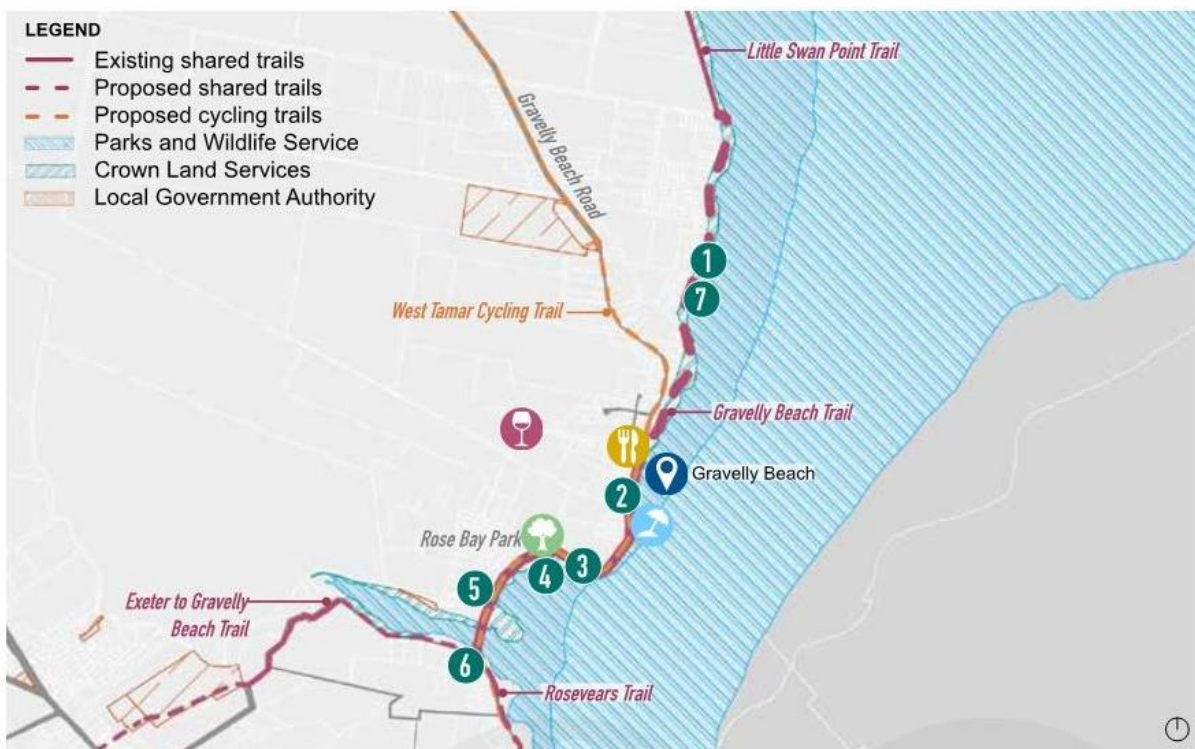
- Council roads**
- Asphalt
  - Chip seal
  - Gravel
  - Other

- Other roads**
- State roads
  - Other (inc forestry)

## West Tamar Trail Strategy extracts – Gravelly Beach Trail and Exeter to Gravelly Beach Trail

### 8.14 GRAVELLY BEACH TRAIL

<b>Trail length:</b>	2.4km
<b>Trail type:</b>	Shared Trail
<b>Location:</b>	Located between the foreshore and Gravelly Beach Road from the Little Swan Point track in the north to Stony Brook Road in the south.
<b>Land owner/ manager:</b>	Crown Land Services
<b>Nearby trail connections:</b>	Little Swan Point, Exeter to Gravelly Beach and the Rosevears Trails.
<b>Issues and opportunities:</b>	The Gravelly Beach Trail would provide a safe and legible shared path along the foreshore connecting the shops and restaurants of the township as well as providing a vital link to the shared use connection to Exeter and Swan Point to the north and Launceston (via Rosevears, Legana and Riverside) in the south. Development of this trail should be considered in any potential future upgrades to Beach Road, Gravelly Beach Road and the implementation of the Gravelly Beach Foreshore Master Plan. Consideration should be given to the trail alignment, surface and width to ensure the protection of vegetation and avoidance of areas subject to inundation.



#### Trail improvement projects:

- 1 Provide a shared trail from the Little Swan Point Trail south along Beach Road to connect to the existing trail on Gravelly Beach Road.
- 2 Upgrade section of trail along Gravelly Beach Road to cater to shared use.
- 3 Provide trail user priority at the Gravelly Beach boat launch.
- 4 Provide wayfinding signage along the trail within Rose Bay Park to clearly define the trail between the park and the existing section of trail along Gravelly Beach Road.
- 5 Upgrade section of trail between Rose Bay Park and Stony Brook Road to cater to shared use.
- 6 Provide wayfinding signage at Junction to the Exeter to Gravelly Beach Link.
- 7 Install signage along the length of the trail to indicate shared use.

## 8.15 EXETER TO GRAVELLY BEACH TRAIL

<b>Trail length:</b>	1km
<b>Trail type:</b>	Shared Trail
<b>Location:</b>	Beginning at the corner of Stony Brook Road and Gravelly Beach Road, the trail continues through a reserve along Stony Brook to Glen Ard Mohr Road connecting Gravelly Beach to Exeter.
<b>Land owner/ manager:</b>	WTC, Private land and Crown Land Services
<b>Nearby trail connections:</b>	Gravelly Beach Trail, West Tamar Cycling Trail, Rosevears Trail
<b>Issues and opportunities:</b>	Development of this trail would provide shared access between Gravelly Beach and Exeter. It would also open up access to key destinations such as the Exeter Community Hub and the commercial centres of both townships.

### LEGEND

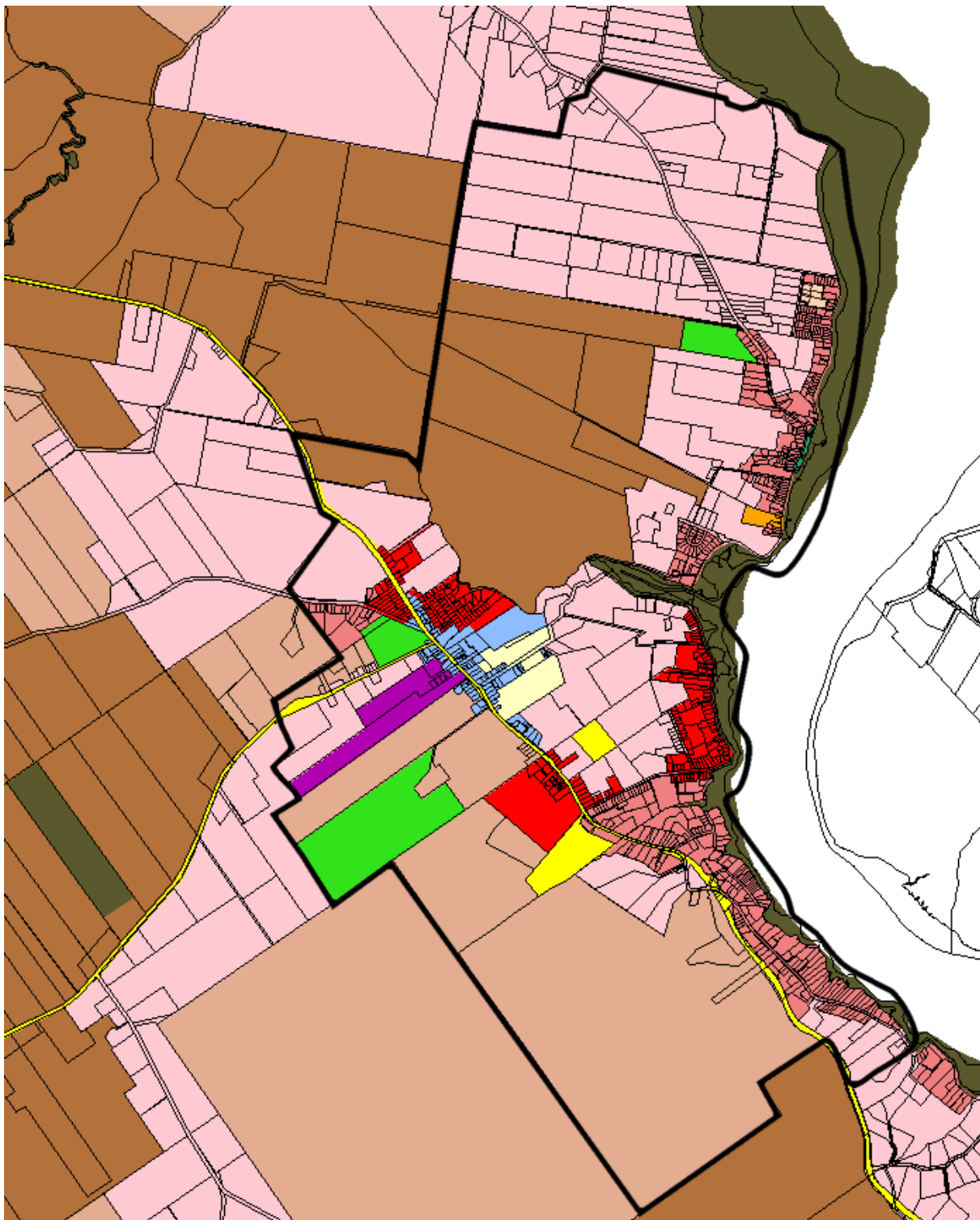
	Existing shared trails
	Proposed shared trails
	Proposed cycling trails
	Parks and Wildlife Service
	Crown Land Services
	Local Government Authority



### Trail improvement projects:

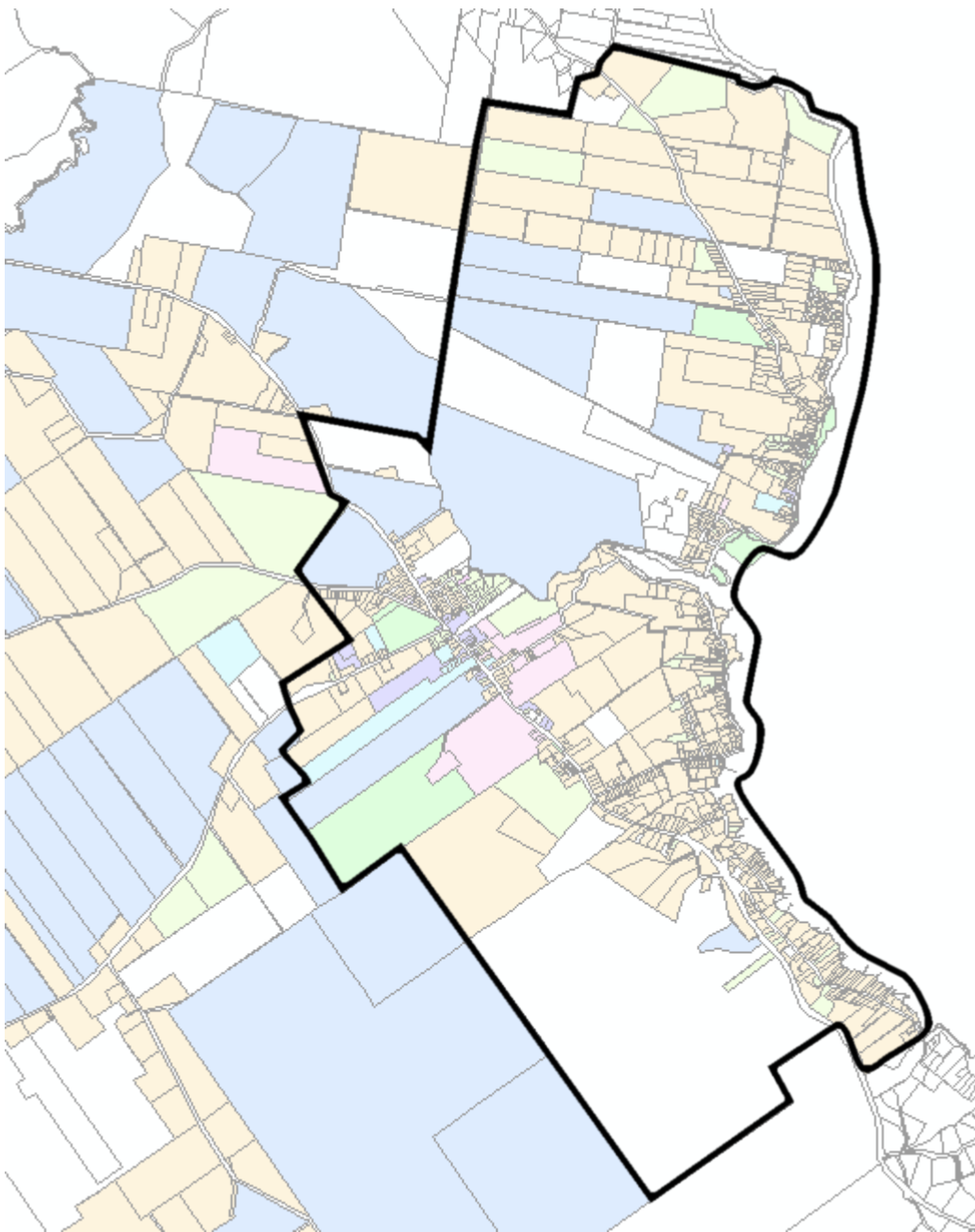
- 1 Provide a shared trail along Stony Brook Road to connect the Exeter Gravelly Beach Link to the Gravelly Beach Trail.
- 2 Provide a shared trail from Glen Ard Mohr Road through the local reserve to the connect the Exeter Gravelly Beach Trail to West Tamar Highway and the Exeter Township.
- 3 Provide interpretive and wayfinding signage along the trail, particularly at the start and end points of the trail.
- 4 Consider providing a formal, all abilities path for the extent of the Exeter Gravelly Beach Link.
- 5 Install signage along the length of the trail to indicate shared use.

## West Tamar Local Provisions Schedule zoning



- |  |   |   |
|--|---|---|
| <span style="color: red;">■</span> General Residential Zone      | <span style="color: blue;">■</span> Central Business Zone                   | <span style="color: grey;">■</span> Major Tourism Zone            |
| <span style="color: darkred;">■</span> Inner Residential Zone    | <span style="color: purple;">■</span> Commercial Zone                       | <span style="color: cyan;">■</span> Port and Marine Zone          |
| <span style="color: pink;">■</span> Low Density Residential Zone | <span style="color: magenta;">■</span> Light Industrial Zone                | <span style="color: yellow;">■</span> Utilities Zone              |
| <span style="color: lightpink;">■</span> Rural Living Zone       | <span style="color: darkpurple;">■</span> General Industrial Zone           | <span style="color: lightyellow;">■</span> Community Purpose Zone |
| <span style="color: orange;">■</span> Village Zone               | <span style="color: tan;">■</span> Rural Zone                               | <span style="color: green;">■</span> Recreation Zone              |
| <span style="color: lightgrey;">■</span> Urban Mixed Use Zone    | <span style="color: brown;">■</span> Agriculture Zone                       | <span style="color: darkgreen;">■</span> Open Space Zone          |
| <span style="color: lightblue;">■</span> Local Business Zone     | <span style="color: olive;">■</span> Landscape Conservation Zone            | <span style="color: orange;">■</span> Future Urban Zone           |
| <span style="color: blue;">■</span> General Business Zone        | <span style="color: darkolivegreen;">■</span> Environmental Management Zone | <span style="color: pink;">■</span> Particular Purpose Zone       |

## Current land use



- Commercial
- Community Purpose
- Industry
- Parks and Recreation
- Residential
- Rural
- Utilities
- Vacant

## Appendix 2 - Summary of feedback received during consultation

Consultation was undertaken in March 2021 seeking views about:

- What the community like about Exeter;
- Improvements that could be made; and
- What the community needs.

As the new Structure Plan will be focused on land use planning, the matters raised have been summarised below as those that relate to land use planning and other feedback that will be addressed through other mechanisms.

### Land use planning

1. Increase amount of residential land in Exeter and Gravelly Beach to support the role of the town as a District Centre. With reference to opportunities:
  - Between Exeter and Gravelly Beach;
  - Tomah Place;
  - Between Winkleigh Road and Highway (northern end of Exeter).
2. New road connections:
  - Between Exeter and Gravelly Beach (through Blackwall) to provide a more direct connection;
  - Between the Frankford Road intersection and Murray Street behind the existing shops.
3. Preservation and maintenance of riparian and open space areas, including buffer areas.
4. Sewerage and water infrastructure - capacity to service current demand and future growth.

### Other feedback

#### Business and services

5. A number of submissions raised the need for new businesses or services within Exeter including:
  - Veterinary services;
  - Mechanic;
  - Additional cafes / extended hours;
  - Expanded health and community services;
  - Additional retail offerings such as a plant nursery, clothing;
  - Upgrades to existing facilities;
  - Free camping / caravan areas.
6. More events such as:
  - Farm school open days;
  - More variety at the show;
  - Music events at local venues;
  - Events to coincide with each of the four seasons.

7. Youth activities and spaces, such as:
  - Girls youth group;
  - Events that engage young people;
  - Playground for young children in Exeter;
  - Space for teenagers to be.
8. Community activities and services:
  - Relocation of some facilities (for example to an expanded Community Hub or to the school);
  - Visitor centre relocation / retention at current site;
  - Opportunities to support social / community gatherings;
  - Exercise classes;
  - Mail delivery;
  - Long term plan for Council depot;
  - Recognition of historical significance of places and people.
9. Increase community and visitor awareness about services and events.

#### Traffic and car parking

10. Traffic circulation in the business area and safety in turning onto or from the Main Road including intersections with:
  - Frankford Road (and the potential conflict with high numbers of heavy vehicles);
  - Glen Ard Mohr Road (in particular access to the Primary and High Schools and signage to limit accidental use by those going to Gravelly Beach);
  - Murray Street.
11. Options for managing traffic impacts:
  - a parallel circulating road through the back street providing a way to turn around rather than doing a u-turn in Main Road;
  - roundabouts at Frankford Road and Glen Ard Mohr Road intersections;
  - bypass of Exeter;
  - Alternative traffic routes for heavy vehicles;
  - Affordable, reliable public transport.
12. Upgrading Gravelly Beach Road and Rosevears Drive.
13. Improving pedestrian / cycling connectivity by:
  - Lowering the speed limit to 40km/hr in the business area;
  - Improving pedestrian paths around the townships, particular between the retirement village and within the Exeter business area and to the Gravelly Beach foreshore;
  - Upgrading the goat track between Exeter and Gravelly Beach.
14. Providing additional safe crossing points:
  - Murray Street intersection;
  - In the Main street / business area;
  - Consider elderly and less mobile;
  - Considering the location of on-street car parking and safe crossing points;
  - Reviewing the location of the crossing in Glen Ard Mohr Road.

15. The need for more car parking in the business area and:
  - Time limits in the main street;
  - Off street public parking area;
  - Park and ride / all day parking;
  - Accessible parking;
  - Additional car parking at the Community Hub.
16. Tresca driveway (which also provides access to the golf club) and car parking requires upgrading.
17. Visibility at the Murray Street / Main Road intersection and the location of the bin on the footpath.  
*Note that the bin has since been relocated*

### Parks and recreation

18. Enhanced park facilities:
  - Children's play park;
  - Skate park / pump track / BMX track;
  - Spaces to stop, have lunch;
  - Community tennis court;
  - Facilities for young people indoor / outdoor;
  - Community gym;
  - Community garden.
19. Park on Main Road (near the library):
  - Community memorial wall;
  - Historical information such as a mural and information about local bushrangers;
  - Playground;
  - Potential to be a real feature;
  - Potential to work with community groups like Rotary;
20. Streetscape improvements for community and to attract tourists:
  - Plant more trees for shade;
  - Improvements in the main street / leafy main street;
  - More colourful plants;
  - In front of the pub is very tired;
  - Put powerlines in main street underground.
21. Natural environment reserve behind Community Hub and linking to the Goat Track
22. Fenced dog park  
*Note: a fenced dog exercise area has been established behind the Exeter Community Hub*
23. Gravelly Beach foreshore:
  - disagreement and support for the masterplan project;
  - support for the art trail;
  - reinstate the gravelly beach.

24. Upgraded facilities at the recreation ground / show grounds:

- Maintenance of the surface;
- New club rooms, function rooms, toilet facilities;
- Fence around the second oval;
- Flood lights on main oval;
- Clarify public access arrangements.

#### Services

25. Installation of a waste dump point and tank refill station for caravans/motor homes in the Exeter area.

26. Weekly waste collection and green waste bins.

*Note fortnightly green waste service commenced in July 2022*

27. Bottle recycling.

## Appendix 3 – 2014 Structure Plan project update

Projects	Current status
<b>Short term</b>	
<p><b>Development of passive recreation facilities on the Clarke land including re-alignment of the walkway between Glen Ard Mohr Road and Exeter</b> A walking track will encourage walkers currently using Glen Ard Mohr Road between the Goat Track and Exeter to cut through the Clarke land (a 30% shorter route).</p>	<p>✓ In progress. Goat track and footpaths on Glen Ard Mohr Road have been upgraded</p>
<p><b>Business Promotion Signage</b> Provide attractive and informative business and community facilities signage on Frankford Road approximately 800m west of Main Road.</p>	<p>✓ Information signage installed</p>
<p><b>Exeter and Gravelly Beach Advisory Group Branding</b> <i>(previously the Exeter Improvement Committee)</i> Ensure projects are consistent with the rebranding project.</p>	<p>✓ Community branding completed and adopted for completed projects</p>
<p><b>Form-based Controls for Main Road</b> Introduce form – rather than use – based statutory planning controls for property fronting both sides of the Main Road between Frankford Highway and Glen Ard Mohr Road.</p>	<p>✗ Not progressing – the introduction of the Tasmanian Planning Scheme will standardise development controls in relation to the design and siting of buildings</p>
<b>Subject to further work</b>	
<p><b>Exeter Community Precinct</b></p> <ul style="list-style-type: none"> <li>Consolidate community services on the site of the Exeter Recreation Centre /RSL (<i>now the Community Hub</i>), possibly within a new building.</li> <li>Relocation of Tresca Services.</li> <li>Future consideration of also co-locating the library, medical facilities and scouts.</li> </ul>	<p>✓ Exeter Community Hub opened in 2019</p> <p>➡ Subject to external funding</p> <p>➡ Subject to external funding</p>
<p><b>Biloo Street Public Park and Carpark / Turning Area</b></p> <ul style="list-style-type: none"> <li>Develop a public park.</li> <li>Develop a link to the tourist information centre.</li> <li>Upgrade informal parking at 57 Main Road (Visitor Centre).</li> </ul>	<p>✓ Public park completed</p> <p>➡ Not completed</p> <p>➡ Subject to future planning</p>
<p><b>Better utilise the RV Overnight Park at Gravelly Beach</b> Increase maximum stay from overnight only.</p>	<p>✗ Legislation does not permit more than an overnight stay</p>
<p><b>Primary Producer Incubator</b> Provide space for small primary producers to process goods, potentially including warehouse and retail space (at the Community Hub or within the precinct)</p>	<p>✓ Kitchen established as part of the Community Hub Other subject to external funding and business case</p>
<p><b>Exeter Showgrounds</b> Discuss with the Exeter Show Society public access to the Showgrounds. Current showground operations including the Exeter Show would continue unchanged.</p>	<p>➡ To be determined by the Exeter Show Society</p>
<b>Future options</b>	
<p><b>Option to purchase the vacant land south of The Exeter Hotel (122 Main Road, Exeter)</b></p> <p>Council to consider future purchase of site to enable better land management, control over future option for a road link and development and ownership of existing car park.</p>	<p>➡ Subject to future consideration – Remains in private ownership</p>
<p><b>Road link to Gravelly Beach through 185 Gravelly Beach Road, Blackwall</b></p> <p>Support rezoning of this land to low density residential, provided that the subdivision layout provides for public road access through the lot towards Glen Ard Mohr Road.</p>	<p>➡ Currently included in the Rural Living Zone. An application has not been made to change the zoning of the land</p>



**Submissions can be made in writing until 30 April 2023**

Please send your submission to West Tamar Council by email to [wtc@wtc.tas.gov.au](mailto:wtc@wtc.tas.gov.au) or by post to PO Box 16, Riverside TAS 7250